



New South Wales

# Road Transport (Mass, Loading and Access) Regulation 1996

under the

Roads Act 1993

His Excellency the Governor, with the advice of the Executive Council, has made the following Regulation under the *Roads Act 1993*.

MICHAEL KNIGHT, MP

Minister for Roads

## Explanatory note

The objects of this Regulation are:

- (a) to implement provisions that are to be applied uniformly within Australia regulating:
  - (i) the mass and loading of vehicles and combinations, and
  - (ii) the conditions for access to roads of vehicles and combinations that are too large or too heavy to be allowed general road access, and
  - (iii) the conditions under which oversize or overmass vehicles and combinations exempted from normal dimension or mass limits may travel on roads and road related areas, and
- (b) to repeal consequentially the *Roads (Weight of Loads on Main Roads) Transitional Regulation 1993*, the *Roads (Weight of Loads on Roads other than Main Roads) Transitional Regulation 1993* and the *Roads (Excess Vehicle Weight Permit) Transitional Regulation 1993*.

This Regulation comprises or relates to matters arising under legislation that is substantially uniform or complementary with legislation of the Commonwealth or another State or Territory.

This Regulation is made under the *Roads Act 1993*, in particular sections 230, 264 (the general regulation-making power) and 264A.

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## Road Transport (Mass, Loading and Access) Regulation 1996

### Part 1 Preliminary

#### 1 Name of Regulation

This Regulation is the *Road Transport (Mass, Loading and Access) Regulation 1996*.

#### 2 Commencement

This Regulation commences on 1 July 1996.

#### 3 Definitions

Expressions used in this Regulation that are defined in the dictionary at the end of this Regulation have the meanings set out in that dictionary.

#### 4 Objects

The objects of this Regulation are:

- (a) to implement provisions that are to be applied uniformly within Australia regulating:
  - (i) the mass and loading of vehicles and combinations, and
  - (ii) the conditions for access to roads of vehicles and combinations that are too large or too heavy to be allowed general road access, and
  - (iii) the conditions under which oversize or overmass vehicles and combinations exempted from normal dimension or mass limits may travel on roads and road related areas, and

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Part 1 Preliminary

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- (b) to repeal consequentially the *Roads (Weight of Loads on Main Roads) Transitional Regulation 1993*, the *Roads (Weight of Loads on Roads other than Main Roads) Transitional Regulation 1993* and the *Roads (Excess Vehicle Weight Permit) Transitional Regulation 1993*.

### 5 Application

- (1) This Regulation applies to the following vehicles and combinations:
  - (a) any vehicle having a GVM exceeding 4.5 tonnes, or
  - (b) any combination having a GCM exceeding 4.5 tonnes.
- (2) This Regulation applies to any such vehicle or combination if it is on a road or a road related area.
- (3) This Regulation does not apply to any such vehicle or combination that is used only on a railway or tramway.

### 6 Notes and diagrams

Except where a contrary intention is indicated, the explanatory note, table of contents, diagrams and notes in the text of this Regulation do not form part of this Regulation.

## **Part 2      Mass, dimension and loading                  requirements for heavy vehicles**

### **7 Heavy vehicles to comply with Schedule 1**

Schedule 1 applies to:

- (a) a vehicle with a GVM exceeding 4.5 tonnes, and
- (b) a combination with a GCM exceeding 4.5 tonnes or one that includes a vehicle with a GVM exceeding 4.5 tonnes, and
- (c) a load on a vehicle described in paragraph (a) or on a combination described in paragraph (b).

### **8 Declaring buses to be complying vehicles**

- (1) This Part applies to a bus that:
  - (a) is not fitted with a compliance plate in accordance with the *Motor Vehicles Standards Act 1989* of the Commonwealth, or
  - (b) is fitted with a compliance plate in accordance with that Act, but the compliance plate indicates that the bus was manufactured before 1 July 1994.
- (2) The Authority may declare that a bus to which this clause applies is a complying bus for the purposes of this Regulation if the Authority is satisfied that the bus meets:
  - (a) the emergency exit specifications in ADR 44, and
  - (b) the rollover strength specifications in ADR 59, and
  - (c) the occupation protection specifications in ADR 68, and
  - (d) is equipped with an approved air suspension system.

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Clause 9 Road Transport (Mass, Loading and Access) Regulation 1996

Part 3 Exemptions from the mass and dimension requirements  
Division 1

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### Part 3 Exemptions from the mass and dimension requirements

#### Division 1 Class 1 Vehicles

##### 9 Application of this Division

- (1) This Division and Schedule 2 apply to:
  - (a) a special purpose vehicle, and
  - (b) an agricultural machine or agricultural implement, and
  - (c) a vehicle or combination (including a low loader or load platform combination) that is specially designed for the carriage of a large indivisible item or is carrying a large indivisible item,  
  
that, together with any load, exceeds:
    - (d) a mass limit in Schedule 1, or
    - (e) a dimension limit in Schedule 1 or the *Motor Traffic Regulations 1935* (other than a dimension limit from which it has been exempted under paragraph 5A of Schedule F to the *Motor Traffic Regulations 1935*).
- (2) This Division and Schedule 2 also apply to a vehicle that is being used as a pilot vehicle or escort vehicle.
- (3) This Division and Schedule 2 do not apply to a vehicle or combination:
  - (a) that is a road train or B-double, or
  - (b) that is carrying a loaded or empty freight container designed for multi-modal transport.

##### 10 Exemption by notice in Gazette

The Authority may, by notice published in the Gazette, exempt Class 1 vehicles or combinations from:

- (a) a mass or dimension requirement in Schedule 1 other than a requirement that relates to a GVM, GCM or manufacturer's limit, or
- (b) a dimension limit in the *Motor Traffic Regulations 1935*.



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**Note.** "Gazette" is defined in the *Interpretation Act 1987* to mean the New South Wales Government Gazette.

Regulation 123AB of the *Motor Traffic Regulations 1935* provides that a person or vehicle is exempted from a dimension limit or any other requirement of those Regulations if that person or vehicle is exempted from that limit or requirement under this Regulation.

## 11 What must be included in a Class 1 notice?

- (1) A Class 1 notice must specify:
  - (a) the class of vehicles or combinations to which it applies, and
  - (b) the areas or routes to which it applies, and
  - (c) the provisions of Schedule 1 and the Motor Traffic Regulations 1935 from which exemption is given, and
  - (d) the conditions under which the exemption is given, and
  - (e) how long it is to remain in force.
- (2) The conditions of a Class 1 notice must include the conditions set out in Schedule 2 that are applicable to the class of vehicles or combinations to which the notice applies.
- (3) The conditions in Schedule 2 may be incorporated in the notice by referring to them rather than by setting them out in full.

## 12 Exemption by permit

- (1) An owner of a vehicle or combination may apply to the Authority for a permit exempting the vehicle or combination from a requirement of Schedule 1 or the *Motor Traffic Regulations 1935*.
- (2) An application for a Class 1 permit must be in writing and in a form approved by the Authority.
- (3) The Authority may grant a Class 1 permit exempting a vehicle or combination from:
  - (a) a mass or dimension requirement of Schedule 1, other than a requirement that relates to a GVM, GCM or manufacturer's limit, or
  - (b) a mass or dimension limit in the Motor Traffic Regulations 1935, other than a dimension limit from which it has been exempted under paragraph 5A of Schedule F to those Regulations.

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Part 3 Exemptions from the mass and dimension requirements  
Division 1

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- (4) Despite subclause (3) (a), a Class 1 permit may exempt a combination consisting of more than one prime mover or hauling unit from compliance with the individual GCMs of the prime movers or hauling units on condition that the sum of the individual GCMs is not exceeded.

**Note.** Regulation 123AB of the *Motor Traffic Regulations 1935* provide that a person or vehicle is exempted from a dimension limit or any other requirement of those Regulations if that person or vehicle is exempted from that limit or requirement under this Regulation.

### 13 What must be included in a Class 1 permit?

- (1) A Class 1 permit must specify:
- (a) the vehicle or combination to which it applies, and
  - (b) the areas or routes to which it applies, and
  - (c) the provisions of Schedule 1 or the *Motor Traffic Regulations 1935* from which exemption is given, and
  - (d) the conditions under which the exemption is given, and
  - (e) how long it is to remain in force.
- (2) Without limiting subclause (1), the conditions of a Class 1 permit must include all the conditions set out in Parts 6 and 7 of Schedule 2, which may be incorporated in the permit by referring to them rather than by setting them out in full.

### 14 Areas and roads

For the purposes of a Class 1 notice or permit, the Authority may designate categories of areas and roads, and, in particular, it may determine that an area or road or part of a road is in category 1, 2 or 3 in Table 7 of Schedule 2.

## Division 2 Class 2 Vehicles

### 15 Application of this Division

This Division applies only to Class 2 vehicles.

**Note.** The axle load limits specified in Schedule 1 also apply to Class 2 vehicles.

**16 Notice or permit required**

A person must not drive or operate a Class 2 vehicle except in accordance with a notice issued, or a permit granted, under this Division.

Maximum penalty: 30 penalty units

**17 Class 2 notice**

The Authority may, by notice published in the Gazette, specify areas and routes in or on which all Class 2 vehicles, or specified categories of Class 2 vehicle, may operate.

**18 Class 2 permit**

- (1) An owner of a Class 2 vehicle may apply to the Authority for a permit to operate a Class 2 vehicle in an area or on a route that is not included in a Class 2 notice.
- (2) An application for a permit must be in writing and in a form approved by the Authority.

**19 Power to grant Class 2 permit**

The Authority may grant a permit to operate a Class 2 vehicle in an area or on a route that is not included in a Class 2 notice.

**20 Conditions of a Class 2 notice or permit**

- (1) The Authority may issue a Class 2 notice or grant a Class 2 permit subject to a condition relating to any or all of the following:
  - (a) the areas or routes to which it applies,
  - (b) if the route includes a bridge, culvert, causeway or road-ferry—the speed at which that any part of that route may be used or entered, the portion of any part of that route to be used and the absence of other traffic before such use or entry,
  - (c) time of day during which the vehicle is not permitted to operate,
  - (d) in the case of a combination— axle spacings,
  - (e) in the case of a road train— the total mass limit of the road train.

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Part 3 Exemptions from the mass and dimension requirements  
Division 2

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- (2) The routes may be designated by reference to a map published by the Authority from time to time or by notice published in the Gazette by the Authority.

**Note.** Maps will be made available at RTA offices.

- (3) The routes designated in the maps referred to may also be varied by notice published in the Gazette by the Authority.

### 21 What must be included in a Class 2 notice or permit?

- (1) The Authority must set out in a Class 2 notice or a Class 2 permit:
- (a) the Class 2 vehicles to which it applies, and
  - (b) the conditions of the notice or permit, and
  - (c) how long the notice or permit is to remain in force, and
  - (d) in the case of a permit—the registration number (if any) of the vehicle to which the permit relates, and
  - (e) in the case of a permit—the name and address of the person to whom the permit is granted.
- (2) A Class 2 permit may be granted for one or more Class 2 vehicles but must, in any case, specify:
- (a) the registration number (if any) of each motor vehicle, or
  - (b) in the case of a combination—the registration number (if any) of the front primemover,
- to which the permit relates.

## Division 3 Class 3 vehicles

### 22 Application of this Division

This Division applies only to Class 3 vehicles.

### 23 Exemption by notice in Gazette

- (1) The Authority may, by notice published in the Gazette, exempt specified categories of Class 3 vehicles from:
- (a) a mass or dimension requirement of Schedule 1, other than a requirement that relates to a GVM, GCM or manufacturer's limit, or
  - (b) a dimension limit in the *Motor Traffic Regulations 1935*.

- (2) The Authority may issue a Class 3 notice subject to conditions, including conditions as to the areas or routes to which the notice applies.
- (3) The Authority may, by notice published in the Gazette, specify areas and routes in or on which all Class 3 vehicles, or specified categories of Class 3 vehicle, may operate.

**Note.** Regulation 123AB of the *Motor Traffic Regulations 1935* provides that a person or vehicle is exempted from a dimension limit or any other requirement of those Regulations if that person or vehicle is exempted from that limit or requirement under this Regulation.

#### **24 Exemption by permit**

- (1) An owner of a Class 3 vehicle may apply to the Authority for a permit exempting the Class 3 vehicle from a requirement of Schedule 1 or the *Motor Traffic Regulations 1935*.
- (2) An application for a permit must be in writing and in a form approved by the Authority.

#### **25 Power to grant Class 3 permit**

The Authority may grant a permit exempting one or more Class 3 vehicles from:

- (a) a mass or dimension requirement of Schedule 1, other than a requirement that relates to a GVM, GCM or manufacturer's limit, or
- (b) a dimension limit in the *Motor Traffic Regulations 1935*.

#### **26 What must be included in a Class 3 notice or permit?**

- (1) The Authority must set out in a Class 3 notice or a Class 3 permit:
  - (a) the Class 3 vehicles to which it applies, and
  - (b) the provisions of Schedule 1 and the *Motor Traffic Regulations 1935* from which exemption is given, and

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Part 3 Exemptions from the mass and dimension requirements  
Division 3

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- (c) the conditions of the notice or permit, and
  - (d) in the case of a permit—the registration number (if any) of the vehicle to which the permit relates, and
  - (e) how long it is to remain in force, and
  - (f) in the case of a permit—the name and address of the person to whom the permit is granted.
- (2) A Class 3 permit may be granted for one or more Class 3 vehicles but must, in any case, specify:
- (a) the registration number (if any) of each motor vehicle, or
  - (b) in the case of a combination—the registration number (if any) of the front primemover,
- to which the permit relates.

### 27 Issuing a Class 3 notice or permit

- (1) Except as provided by subclauses (2)–(4), the conditions of a Class 3 notice or Class 3 permit must ensure that a single motor vehicle, or a combination, operating under the notice or permit is so configured and loaded that:
- (a) the mass on each single axle and axle group is not more than 10% in excess of the limit specified in Schedule 1 for that single axle or axle group, and
  - (b) a dimension limit in Schedule 1 and the *Motor Traffic Regulations 1935* is not exceeded by more than 10%.
- (2) In the case of a combination, if each of the individual vehicles that make up the combination, together with any load, comply in all respects with the mass and dimension limits of Schedule 1 and the *Motor Traffic Regulations 1935*, the conditions of a Class 3 notice or Class 3 permit may allow an increase in the total length limit or the total mass limit, or both, prescribed in that Schedule and those Regulations for the combination as a whole.
- (3) Subclause (1) does not apply to a Class 3 notice or a Class 3 permit if:
- (a) the travel under the notice or permit:
    - (i) is connected with the operation of a particular facility such as a mine or port, or
    - (ii) takes place mainly on roads the primary purpose of which is to serve the needs of such a facility, and

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- (b) either:
- (i) the notice or permit is not likely to affect road transport to, from or within another State or Territory, or
  - (ii) the States and Territories likely to be affected have reached agreement on the terms of the notice or permit.
- (4) Subclause (1) does not apply to a Class 3 notice or a Class 3 permit that relates to the operation of a B-double or road train in the following circumstances:
- (a) a trailer forming part of the B-double or road train is carrying one or more large indivisible items, and
  - (b) the carrying of more than one large indivisible item does not cause the vehicle or combination and its load to exceed a dimension limit in Schedule 1 or the *Motor Traffic Regulations 1935* that would not have been exceeded by the carrying of only one of the large indivisible items, and
  - (c) but for the large indivisible items, the B-double or road train and any load being carried would comply with the mass and dimension limits in Schedule 1.
- (5) If a Class 3 notice or Class 3 permit is likely to affect road transport to, from or within another State or Territory, the Authority must consult with the corresponding Authority of that other State or Territory and must issue the notice or grant the permit only on conditions that are agreed with that corresponding Authority.
- (6) The Authority must give particulars, in writing, to the corresponding Authority of each other State and Territory and to the National Road Transport Commission of any scheme under which a Class 3 notice or Class 3 permit is to remain in force for more than 6 months.
- (7) The particulars referred to in subclause (6) must be given within 28 days of the publication of the notice or grant of the permit.

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Part 4 General

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### **Part 4 General**

#### **28 Application of Part**

Unless a contrary intention appears, this Part applies to Class 1, Class 2 and Class 3 Vehicles.

#### **29 Defence to a prosecution**

- (1) It is a defence to a prosecution for an offence under this Regulation (other than an offence to which subclause (2) applies) if the defendant proves that the offence:
  - (a) was the result of an accident, or
  - (b) was the result of compliance with a direction of a police officer, or
  - (c) could not have been avoided by any reasonable efforts on the part of the defendant.
- (2) Section 235 of the Act applies to any offence under this Regulation relating to a breach of a mass limit in the same way as it applies to an offence under that section.

#### **30 Keeping documents**

- (1) The driver of a vehicle or a combination must carry in the driving compartment:
  - (a) a copy of any notice or permit under which the vehicle or the combination is operating, or
  - (b) an information sheet issued by the Authority setting out the obligations imposed under the notice.

Maximum penalty: 30 penalty units

- (2) Subclause (1) does not apply if the notice or permit states that the subclause does not apply.

#### **31 Failure to comply with Schedule 1—motor vehicles**

- (1) If a motor vehicle or a load on a motor vehicle does not comply with a requirement in Schedule 1 that applies to it, each of the following persons is guilty of an offence:
  - (a) the owner of the motor vehicle,
  - (b) the driver of the motor vehicle.

Maximum penalty: 30 penalty units



- (2) This clause applies to any vehicle to which Schedule 1 applies (or would apply but for any exemption), whether or not it is a Class 1, 2 or 3 vehicle.

### **32 Failure to comply with Schedule 1—trailers**

- (1) If a trailer or a load on a trailer does not comply with a requirement in Schedule 1 that applies to it, each of the following persons is guilty of an offence:
- (a) the owner of the trailer,
  - (b) the owner of any motor vehicle towing the trailer,
  - (c) the driver of any motor vehicle towing the trailer.

Maximum penalty: 30 penalty units

- (2) This clause applies to any vehicle to which Schedule 1 applies (or would apply but for any exemption), whether or not it is a Class 1, 2 or 3 vehicle.

### **33 Failure to comply with Schedule 1—combinations**

- (1) If a combination fails to comply with a requirement in Schedule 1 that applies to it, each of the following persons is guilty of an offence:
- (a) the owner of the motor vehicle included in the combination,
  - (b) the driver of the motor vehicle included in the combination,
  - (c) the owner of a trailer included in the combination.

Maximum penalty: 30 penalty units

- (2) This clause applies to any vehicle to which Schedule I applies (or would apply but for any exemption), whether or not it is a Class 1, 2 or 3 vehicle.

### **34 Punishment for failure to comply with this Regulation**

- (1) A person who is both the driver and owner of the relevant vehicle or trailer may be punished only once for the same failure of the combination to comply with a requirement of or imposed under this Regulation.

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Clause 34 Road Transport (Mass, Loading and Access) Regulation 1996

Part 4 General

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- (2) If:
- (a) a person is prosecuted for an offence under this Regulation, and
  - (b) the offence relates to the failure of a vehicle or a combination to comply with a mass or dimension limit specified in Schedule 1, and
  - (c) an exemption by notice or permit has been given under this Regulation on condition that the vehicle or combination comply with a greater mass or dimension limit specified in that notice or permit, and
  - (d) at the time of the offence the vehicle or combination was travelling:
    - (i) on a route other than a route on which it was permitted to travel under the conditions of the exemption, or
    - (ii) at a time other than a time at which it was permitted to travel under the conditions of the exemption, or
    - (iii) accompanied by fewer than the number of pilot or escort vehicles required under the conditions of the exemption, or
    - (iv) with an aggregate mass that is more than 10% in excess of the limit allowed under the conditions of the exemption,

the exemption must be disregarded for the purposes of the prosecution.

### 35 Loading offences

- (1) A load on a vehicle must not be placed in a way that makes the vehicle unstable or unsafe.
- (2) A load on a vehicle must be secured so that it is unlikely to fall or be dislodged from the vehicle.
- (3) An appropriate method must be used to restrain the load on a vehicle.

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- (4) If a motor vehicle or a load on a motor vehicle does not comply with a loading requirement under this Regulation that applies to it, each of the following persons is guilty of an offence:
- (a) the owner of the motor vehicle,
  - (b) the driver of the motor vehicle.
- (5) If a trailer or a load on a trailer does not comply with a loading requirement under this Regulation, each of the following persons is guilty of an offence:
- (a) the owner of the trailer,
  - (b) the owner of any motor vehicle towing the trailer,
  - (c) the driver of any motor vehicle towing the trailer.
- (6) If a combination does not comply with a loading requirement under this Regulation that applies to it, each of the following persons is guilty of an offence:
- (a) the owner of the motor vehicle included in the combination,
  - (b) the driver of the motor vehicle included in the combination,
  - (c) the owner of a trailer included in the combination.

Maximum penalty (subclauses (1)–(6)): 30 penalty units

- (7) Subclauses (4)–(6) apply to any vehicle to which Schedule 1 applies (or would apply but for any exemption).
- (8) In proceedings for a failure to comply with this clause, it is sufficient for the prosecution to prove that the load on the vehicle was not placed, secured or restrained (as the case requires) in a way that met the performance standards recommended in the *Load Restraint Guide—Guidelines for the safe carriage of loads on road vehicles* published by the Australian Government Publishing Service on 12 December 1994.

**Note.** Copies of the *Load Restraint Guide—Guidelines for the safe carriage of loads on road vehicles* are available from Commonwealth Government Bookshops, the Federal Office of Road Safety or the Roads and Traffic Authority.

- (9) In proceedings for a failure to comply with this clause, a document purporting to be the *Load Restraint Guide* referred to subclause (8) must be taken to be the *Load Restraint Guide*, unless the document is proved by the defendant not to be the *Load Restraint Guide*.

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Clause 35 Road Transport (Mass, Loading and Access) Regulation 1996

Part 4 General

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- (10) If the prosecution in proceedings for a failure to comply with subclause (2) (relating to securing a load on a vehicle) proves that the load, or part of the load, had fallen off the vehicle, the burden of proof is on the defendant to show compliance.
- (11) This clause applies to a vehicle even if it is not a Class 1, 2 or 3 vehicle.

### **36 Failure to comply with a condition of a Class 1, 2 or 3 notice or permit**

- (1) A person is guilty of an offence if a condition of a notice or permit (or of an exemption granted under clause 39) is breached by:
  - (a) the person, or
  - (b) a combination that the person is driving, or
  - (c) a motor vehicle that does not form part of a combination and that the person is driving, or
  - (d) a vehicle forming part of a combination that the person is driving, or
  - (e) in the case of the owner of any such motor vehicle, vehicle or combination—the owner if the owner allows it to be driven by another person.

Maximum penalty: 30 penalty units

- (2) Despite subclause (1), the driver or owner of a Class 1 vehicle or combination is not liable to prosecution for contravening Schedule 2, but may be liable to prosecution for contravening Schedule 1 or the *Motor Traffic Regulations 1935* if the vehicle or combination breaches a condition of a notice or permit (or of an exemption granted under clause 39) by travelling:
  - (a) in an area or on a route other than an area or a route on which it is permitted to travel under a notice or permit, or
  - (b) at a time other than a time at which it is permitted to travel under a notice or permit, or
  - (c) accompanied by fewer than the number of pilot or escort vehicles required under a notice or permit that applies to it, or
  - (d) with an aggregate mass that is more than 10% in excess of the limit allowed under a notice or permit, or

- (e) with an axle load that exceeds the load applicable under Schedule 2 to an axle or group of axles.
- (3) A person is guilty of an offence if a condition of a Class 3 notice or Class 3 permit (or of an exemption granted under clause 39) is not met by:
- (a) the person, or
  - (b) a combination which the person is driving or operating, or
  - (c) a single motor vehicle which the person is driving or operating.

Maximum penalty: 30 penalty units

- (4) The driver or owner of a Class 3 vehicle is not liable to prosecution under Part 3, but may be liable to prosecution for contravening Schedule 1 or the *Motor Traffic Regulations 1935* if the Class 3 vehicle breaks a condition of a notice or permit (or of an exemption granted under clause 39) by travelling:
- (a) in an area or on a route other than an area or route in or on which it is permitted to travel under a notice or permit, or
  - (b) at a time other than a time at which it is permitted to travel under a notice or permit, or
  - (c) with an aggregate mass that is more than 10% in excess of the limit allowed under a notice or permit, or
  - (d) with an axle load that exceeds the load applicable under Schedule 2 to an axle or group of axles.
- (5) Any notice or permit issued under this Regulation must be disregarded for the purposes of a prosecution for contravening Schedule 1 or the *Motor Traffic Regulations 1935* in the circumstances referred to in subclauses (2) and (4).
- (6) This clause applies to a vehicle even if it is not a Class 1, 2 or 3 vehicle.

**Note.** Regulation 123AB of the *Motor Traffic Regulations 1935* provides that a person or vehicle is exempted from a dimension limit or any other requirement of those Regulations if that person or vehicle is exempted from that limit or requirement under this Regulation.

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Clause 37 Road Transport (Mass, Loading and Access) Regulation 1996

Part 4 General

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### 37 Multiple offences and multiple requirements

- (1) A person who is convicted of an offence in relation to a vehicle or a combination that fails to comply with a requirement in Schedule 1 may be convicted of another offence if different parts of the vehicle or combination simultaneously fail to comply with the same or a similar requirement in the Schedule.
- (2) A person who is both the owner and the driver of the motor vehicle may be punished only once for the same failure of the vehicle or load to comply with a requirement referred to in subclause (1).
- (3) A person who is both the driver and the owner of a vehicle or combination may be punished only once in relation to the same failure of the person, vehicle or combination to comply with a condition of a notice or permit or of an exemption granted under clause 39.
- (4) A person who is both the driver of a pilot vehicle and the owner of a vehicle or combination being accompanied by that pilot vehicle may be prosecuted in either of those capacities in the circumstances described in clause 36 (2) (a), (b) or (c), but may be punished only once in respect of a particular offence arising from those circumstances.
- (5) A person who is convicted of an offence, in relation to a part of a motor vehicle or trailer that fails to comply with a condition, may be convicted of another offence if another part of the motor vehicle or trailer also fails to comply with the same or a similar condition.
- (6) If more than one mass limit applies to a vehicle or combination, or part of a vehicle or combination, the lower mass limit must be complied with.
- (7) This clause applies to a vehicle even if it is not a Class 1, 2 or 3 vehicle.

**38 Failure of a pilot or escort vehicle to comply with a requirement**

- (1) If a pilot vehicle:
  - (a) accompanies an oversize vehicle or combination in any of the circumstances described in clause 36 (2) (a), (b) or (c), or
  - (b) does not comply with a requirement of Part 5 of Schedule 2,its driver, and the owner of the oversize vehicle or combination it is accompanying, are each guilty of an offence.
- (2) If an escort vehicle does not comply with a requirement of Part 5 of Schedule 2, its driver, and the owner of the oversize vehicle or combination it is accompanying, are each guilty of an offence.

Maximum penalty: 30 penalty units

**39 Exemptions in emergencies**

- (1) In an emergency such as a fire, explosion or natural disaster, the Authority may exempt a vehicle or combination, or its driver or owner, from a requirement of this Regulation if
  - (a) the vehicle or combination is being used, or is intended to be used, to protect life or property, or to restore communication or the supply of energy or water or services such as sewage disposal, and
  - (b) the exemption does not present an unreasonable danger to other road users.
- (2) In an emergency such as a fire, explosion or natural disaster (including a drought), the Authority may exempt a single motor vehicle or a combination, or its driver or owner, from a requirement of this Regulation if the Authority is satisfied that:
  - (a) the exemption will not result in an unreasonable danger to other road users, and
  - (b) the single motor vehicle or the combination is being used, or is intended to be used, to protect life or property, or to restore communication or the supply of energy or water or services such as sewage disposal, or to provide drought relief.

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Clause 39 Road Transport (Mass, Loading and Access) Regulation 1996

Part 4 General

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- (3) An exemption may be subject to conditions imposed by the Authority.
- (4) The Authority must make a written record of the exemption, and any conditions to which it is subject, but may cause it to be communicated orally to the owner or driver.



## **Part 5 Special load limits**

### **Division 1 Light traffic thoroughfares**

#### **40 Loads on light traffic thoroughfares**

- (1) For the purposes of section 112 of the Act, any notice that is required to be conspicuously displayed, must be in or similar to the form illustrated in Schedule 3.
- (2) A notice of the kind illustrated in that Schedule prohibits the passage, from a direction facing the notice, of a vehicle or combination exceeding the total mass indicated by the notice.

### **Division 2 Special load limits**

#### **41 Special load limits**

- (1) In any special case, or where the provisions of this Regulation do not apply to a vehicle, a roads authority may fix a maximum axle load, maximum loaded mass or other load limit to the vehicle.
- (2) Any such load or limit fixed under subclause (1) must be:
  - (a) notified in writing to the owner of the vehicle to which it applies, or
  - (b) published in the Gazette or in a local newspaper circulated in the locality to which the limit applies.
- (3) A limitation referred to in subclause (2) operates as though prescribed by this Regulation and any person who fails to comply with the terms of any such notification issued for the purposes of this clause is guilty of an offence.

Maximum penalty: 30 penalty units

## **1996 No 273**

Clause 42 Road Transport (Mass, Loading and Access) Regulation 1996

Part 6 Mass checks of heavy vehicles  
Division 1

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## **Part 6 Mass checks of heavy vehicles**

### **Division 1 Mass checks**

#### **42 Driver to comply with requirements**

- (1) For the purposes of section 230 of the Act, an authorised officer must, where the officer has been authorised to exercise his or her powers by a roads authority, wear a badge or other distinguishing mark given to him or her by a roads authority for the purpose of indicating his or her authority.
- (2) For the purposes of section 230 (2) of the Act, an authorised officer may direct the driver of a vehicle to stop:
  - (a) by displaying a notice (whether or not it is illuminated) containing at least the word "STOP" and, where the officer has been authorised to exercise his or her powers by a roads authority, the name of that authority, and
  - (b) at a place on a road on or near if there is checking station indicated by a notice clearly displayed up on or near the roadway.
- (3) A notice referred to in this clause may be held by hand, erected on or near a road, or displayed (whether electronically or otherwise) on a vehicle and, if it is an illuminated notice, may be switched on by an officer whether or not the officer is in the immediate vicinity of the sign at the time.
- (4) For the purposes of section 230 of the Act, an authorised officer may also direct a driver of a vehicle to drive the vehicle from the location at which the authorised officer has directed the driver to stop to some other location in the vicinity of the notice referred to in subclause (3).
- (5) An authorised officer may also direct the driver of a vehicle to stop by clearly displaying a notice displaying the words "DIVERT TO HEAVY VEHICLE CHECKING STATION", "DIVERT TO CHECKING STATION" or similar words.

- 
- (6) However, a direction referred to in subclause (5) is to be taken not to have been given if there is displayed in or in the vicinity of the notice referred to in subclause (5) the word “CLOSED”.
  - (7) If only vehicles of a particular class are required to stop for the purposes of this Regulation, the notice referred to in subclause (2) must indicate the GVM mass limit of the vehicles to which the notice applies.

#### **43 Method of determining wheel loads**

- (1) For the purpose of determining the wheel load of a wheel of a motor vehicle or a trailer, the wheel is to be weighed (alone or together with any other wheel or wheels forming an axle group of which the wheel the weight of which is being determined forms part) in such a way that the wheel or wheels are weighed together with the portion of the vehicle and load supported by the wheel or wheels.
- (2) If the wheel load of a wheel is determined under subclause (1) by weighing the wheel with other wheels, the load of the wheel the load of which is being determined is the mass of all the wheels so weighed divided by the number of wheels so weighed.

#### **44 Method of determining axle loads**

For the purpose of determining the axle load of an axle or axle group of a motor vehicle or trailer, either of the following methods may be used:

- (a) the wheel loads of all wheels on the axle or in the axle group may be added together, or
- (b) if the mass of the vehicle together with any load that it supports is known—the axle loads of all single axles and all the axle groups other than the axle or group the load of which is being determined are to be subtracted from that mass.

#### **45 Method of determining total mass**

For the purpose of determining the total mass of a vehicle or combination, either of the following methods may be used:

- (a) the axle loads of all the single axles and all the axle groups of a vehicle or combination may be added together, or

## 1996 No 273

Clause 45 Road Transport (Mass, Loading and Access) Regulation 1996

Part 6 Mass checks of heavy vehicles  
Division 1

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- (b) all the wheels of the vehicle or combination, together with the vehicle and any added load supported by the wheels, may be weighed simultaneously on a weighing device or weighing devices.

### 46 Weighing devices

If a weighing device being used for the purpose of determining a wheel load, an axle load or a total mass in accordance with this Regulation shows a mass in excess of the weight for which the weighing device has been verified under the *Trade Measurement Act 1989*, the load on the weighing device when so used is, for the purposes of this Regulation, to be taken to be the weight for which the weighing device has been so verified.

## Division 2 Variations

### 47 Additional mass restrictions

- (1) A roads authority may, by displaying a notice on or near the road to which the notice relates, prohibit the driving of any vehicle:
  - (a) having an axle load exceeding the appropriate axle load specified in the notice for that class of vehicle, or
  - (b) if the sum of the axle loads of a group of axles of the vehicle exceeds the appropriate sum specified in the notice for that class of vehicle,

on the road or part of the road that is newly formed, constructed or repaired, or has been damaged by flood, submergence, subsidence or otherwise, or in any special case, during the period specified on the notice if, in the opinion of that roads authority, damage is likely to be inflicted on that road.

- (2) An axle load or sum of the axle loads of a group of axles that is specified in the notice referred to in subclause (1) for any class of vehicle must not exceed the appropriate axle load or sum of axle loads specified in Schedule 1.
- (3) A person must not, without reasonable excuse, fail to comply with a prohibition under this clause.

Maximum penalty: 30 penalty units

## **Part 7      Miscellaneous**

### **48    Class 1, 2 and 3 permit application fees**

The fee for the issue of a Class 1, 2 or 3 permit is \$50.

### **49    Repeal, savings, transitional and other provisions**

- (1) The following Regulations are repealed:
  - (a) the Roads (*Excess Vehicle Weight Permit*) Transitional Regulation 1993,
  - (b) the Roads (*Weight of Loads on Main Roads*) Transitional Regulation 1993, and
  - (c) the Roads (*Weight of Loads on Roads Other than Main Roads*) Transitional Regulation 1993.
- (2) Clause 34 of the Roads (*General*) Regulation 1994 is repealed.
- (3) Any act, matter or thing that, immediately before the repeal of a Regulation referred in subclause (1), had effect under that Regulation is taken to have effect under this Regulation.

## **Schedule 1 Mass and loading requirements for heavy vehicles**

(Clause 7)

### **Part 1 Mass limits**

#### **1 Mass limit for a single vehicle**

The total mass of a vehicle and any load must not exceed the vehicle's GVM.

#### **2 Mass limits for tyres, wheels and axles**

- (1) The wheel load or axle load must not exceed the limit set by its manufacturer.
- (2) The mass on a tyre must not exceed the greatest load capacity determined for the tyre by the manufacturer at a cold inflation pressure that does not exceed:
  - (a) 825 kilopascals for a radial ply tyre, or
  - (b) 700 kilopascals for any other tyre.
- (3) The mass on an axle group or single axle must not exceed the limit provided for it in Table 1.
- (4) The mass limit in Table 1 that applies to an axle group that includes a retractable axle must be determined as if the axle did not exist, unless subclause (5) applies.
- (5) A retractable axle is part of an axle group for the purposes of Table 1 if, when the mass on the group exceeds:
  - (a) 6 tonnes, in the case of a tandem axle group, or
  - (b) 11 tonnes, in the case of a tri-axle group,the tyres on the axle are in contact with the ground and the load-sharing suspension system is operating on each axle (including the retractable axle) and tyre in the group.

- (6) The sum of the mass on the axle groups and single axles on a vehicle or combination must not exceed:
- (a) in the case of a complying bus:
    - (i) if the complying bus has two axles—16.0 tonnes, and
    - (ii) if the complying bus has a rear tandem axle group fitted with single tyres on one axle and dual tyres on the other axle—20.0 tonnes, and
    - (iii) if the complying bus has a rear tandem axle group fitted with dual tyres upon both axles—22.5 tonnes, and
  - (b) in any other case—the sum of the mass limits of the axle groups and single axles, as provided in Table 1.
- (7) For the purposes of Table 1, the tyre width of a radial ply tyre is the number of millimetres marked on the tyre in the position labelled “Section width in mm” on the diagram below clause 1 (3) of Schedule 2.
- (8) For the purposes of Table 1, the tyre width of a bias-type tyre is the number of millimetres equal to 25.4 times the number marked on the tyre in the position labelled “Width code in inches” on the diagram below clause 1 (4) of Schedule 2.
- (9) If no section width or width code is marked on a tyre, the tyre width for the purposes of Table 1 may be determined by measuring the width of the part of the tyre that normally comes into contact with the road surface.

**Table 1****Mass limits for single axles and axle groups**

Description of single axle or axle group	Mass Limit (tonnes)
<b>Single axles and single axle groups</b>	
Single steer axle on:	
(a) a complying bus	6.5
(b) any other motor vehicle	6.0

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## Schedule 1 Mass and loading requirements for heavy vehicles

**Table 1—continued****Mass limits for single axles and axle groups**

Description of single axle or axle group	Mass Limit (tonnes)
Single axle or single axle group fitted with single tyres with section width of:	
(a) less than 375 mm	6.0
(b) at least 375 mm but less than 450 mm	6.7
(c) at least 450 mm	7.0
Single axle or single axle group fitted with dual tyres on:	
(a) a pig trailer	8.5
(b) a complying bus or a bus licensed to carry standing passengers	10.0
(c) any other vehicle	9.0
<b>Twinsteer axle groups</b>	
Twinsteer axle group without a load-sharing suspension system	10.0
Twinsteer axle group with a load-sharing suspension system	11.0
<b>Tandem axle groups</b>	
Tandem axle group fitted with single tyres with section width of:	
(a) 375 mm or more	11.0
(b) 375 mm or more but less than 450 mm	13.3
(c) 450 mm or more	14.0
Tandem axle group fitted with single tyres on one axle and dual tyres on the other axle on:	
(a) a complying bus	14.0
(b) any other vehicle	13.0
Tandem axle group fitted with dual tyres on:	
(a) a pig trailer	15.0
(b) any other vehicle	16.5



**Table 1—continued**

<b>Mass limits for single axles and axle groups</b>	
Description of single axle or axle group	Mass Limit (tonnes)
<b>Tri-axle groups</b>	
Tri-axle group on a vehicle fitted with single tyres with section width of less than 375 mm on all axles, or single tyres on 1 or 2 axles and dual tyres on the other axle or axles	15.0
Tri-axle group on a pig trailer with either single tyres with section width of at least 375 mm, dual tyres on all axles, or a combination of those tyres	18.0
Tri-axle group, on a vehicle other than a pig trailer, with either single tyres with section width of at least 375 mm, dual tyres, or a combination of those tyres	20.0
<b>Quad-axle groups</b>	
Quad-axle group fitted with single tyres with section width of less than 375 mm	15.0
Quad-axle group fitted with single tyres with section width of at least 375 mm or dual tyres	20.0

### 3 Mass limits relating to axle spacing

- (1) If the total mass of a vehicle or a combination cannot lawfully exceed 42.5 tonnes, the mass limits in Part 1 of Table 2 must not be exceeded in relation to the distances set out in that Part that apply to the vehicle or combination.
- (2) If the total mass of a vehicle or a combination cannot lawfully exceed 42.5 tonnes, the mass limits in Part 1 of Table 2 apply to the sum of the masses on each axle group or single axle in the distance referred to in that Part, including the axles between which the distance is measured.
- (3) For any B-double, the mass limits in Part 2 of Table 2 must not be exceeded in relation to the distances set out in that Part that apply to the B-double or any vehicle forming part of it.

- (4) For any B-double, the mass limits in Part 2 of Table 2 apply to the sum of the masses of each axle group or single axle in the distance referred to in that Part, including the axles between which the distance is measured.
- (5) For any road train, the mass limit must not exceed that determined or specified by the Authority.
- (6) For any road train that complies with the limits determined or specified in accordance with subclause (5), the mass limits in Part 3 of Table 2 apply to the sum of the masses of each axle group or single axle in the distance referred to in that Part, including the axles between which the distance is measured.

#### 4 Spacing rules

- (1) A B-double with two tri-axle groups must conform to the formulae:

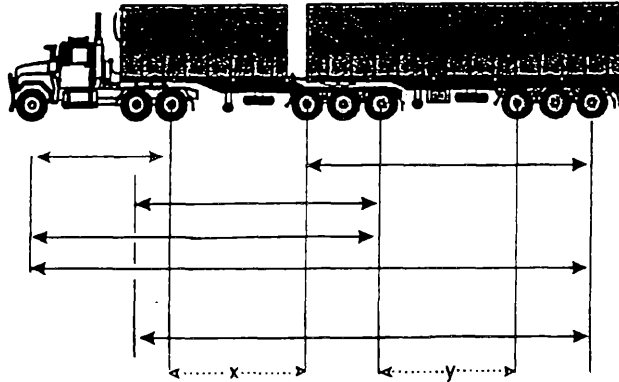
$$x - y \leq 1 \text{ and } y - x \leq 1.3,$$

where

$x$  is the distance in metres between the centres of the closest axles of the second and third axle groups, treating the steer axles as the first axle group and assigning to the next rearmost axle group the description *second axle group* and to each successive axle group a higher ordinal number, and

$y$  is the distance in metres between the centres in the closest axles of the third and fourth axle groups, treating the steer axles as the first axle group and assigning to the next rearmost axle group the description *second axle group* and to each successive axle group a higher ordinal number.

- (2) For B-Doubles other than those referred to in subclause (1), the distance between the centres of tandem axle groups must not be less than 6 m and the distance between the centres of the tandem and tri-axle groups must not be less than 7 m.



- (3) The distance between the axles closest to each other in any adjacent multi-axle groups in a road train must not differ from the distance between the axles closest to each other in any other adjacent multi-axle groups by more than 1 m.
- (4) The total mass of a vehicle or combination, and any load, must not exceed 15 tonnes if the distance between any 2 axles that are not part of the same axle group is less than 2.5 m.

## 5 Measurement of distances for Table 2

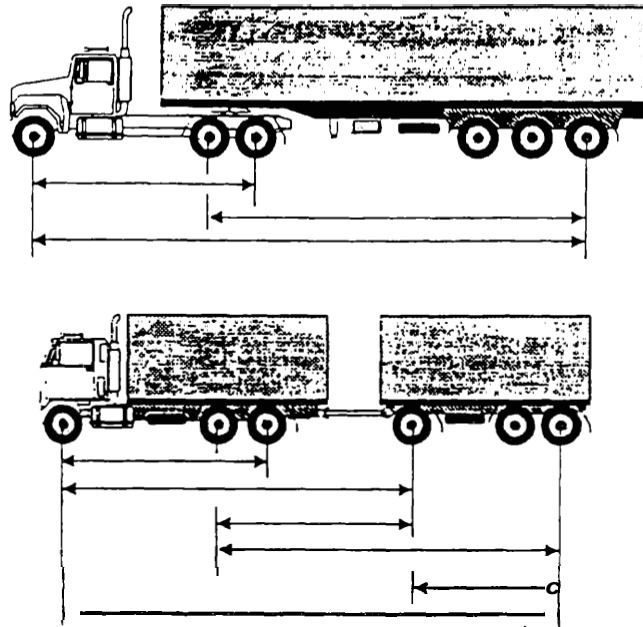
Each distance in Table 2 refers to:

- (a) the distance from the centre of any single axle to the centre of any other single axle, or
- (b) the distance from the centre of any single axle to the centre of the furthest axle in any axle group, or
- (c) the greatest distance between the centres of axles in any 2 axle groups.

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**Table 2**

<b>Part 1—Mass limits relating to axle spacing</b>		
Distance (metres) between extreme		Mass limit (tonnes)
<b>Exceeding</b>	<b>Not exceeding</b>	
0	3.7	23.0
3.7	3.8	23.5
3.8	4.0	24.0
4.0	4.2	24.5
4.2	4.3	25.0
4.3	4.5	25.5
4.5	4.7	26.0
4.7	4.8	26.5
4.8	5.0	27.0
5.0	5.2	27.5
5.2	5.3	28.0
5.3	5.5	28.5

**Table 2—continued****Part 1—Mass limits relating to axle spacing**

Distance (metres) between extreme		Mass limit (tonnes)
<b>Exceeding</b>	<b>Not exceeding</b>	
5.5	5.7	29.0
5.7	5.8	29.5
5.8	6.0	30.0
6.0	6.2	30.5
6.2	6.3	31.0
6.3	6.5	31.5
6.5	6.7	32.0
6.7	6.8	32.5
6.8	7.0	33.0
7.0	7.2	33.5
7.2	7.3	34.0
7.3	7.5	34.5
7.5	7.7	35.0
7.7	7.8	35.5
7.8	8.0	36.0
8.0	8.2	36.5
8.2	8.3	37.0
8.3	8.5	37.5
8.5	8.7	38.0
8.9	8.8	38.5
8.8	9.0	39.0
9.0	9.2	39.5
9.2	9.3	40.0
9.3	9.5	40.5
9.5	9.7	41.0
9.7	9.8	41.5
9.8	10.0	42.0
10.0		42.5

**Part 2—Mass limits relating to axle spacing—B-doubles**

<p>For any B-double, the loaded mass of which can lawfully exceed 42.5 tonnes where the distance between the extreme axles is less than 21 metres, the mass limit must not exceed that calculated in accordance with the formula opposite.</p>	<p><math>M = 1.5L + 29.5</math>                      where                      M is the mass limit in tonnes                      L is the distance in metres</p>
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<p>For any B-double, the loaded mass of which can lawfully exceed 42.5 tonnes where the distance between the extreme axles is not less than 21 metres, the mass limit must not exceed that specified opposite.</p>	<p>62.5 tonnes</p>
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<p>For all B-doubles, the sum of the axle loads in any two or more adjacent axle groups (or of any single axle and the axles in one or more adjacent axle groups) must not exceed that calculated in accordance with the formulae opposite.</p>	<p>For L less than or equal to 11.33 metres, <math>M = 3L + 12.5</math>                      where                      M is the mass limit in tonnes and                      L is the distance in metres.</p>
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For L greater than 11.33 metres  
 $M = 1.5L + 29.5$   
 where  
 M is the mass limit in tonnes and  
 L is the distance in metres.

**Part 3—Mass limits relating to axle spacing—Road trains**

<p>For any road train the sum of the axle loads of the axles in any two or more adjacent axle groups (or of any single axle and the axles in one or more adjacent axle groups) must not exceed that calculated in accordance with the formula opposite.</p>	<p><math>M = 1.5L + 29.5</math>                      where                      M is the mass limit in tonnes, and                      L is the distance in metres</p>
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**6 Mass limits for combinations**

- (1) The total mass of a combination other than a road train or B-Double, and any load, must not exceed 42.5 tonnes.
- (2) The loaded mass of a dog trailer or pig trailer must not exceed the loaded mass of the towing vehicle.
- (3) The total mass of a combination, and any load, must not exceed the towing vehicle's GCM.
- (4) If the manufacturer of a motor vehicle forming part of a road train or B-Double has not determined the GCM of the vehicle, the total mass of the combination and any load must not exceed the number of kilograms worked out using the following formula:

$$\text{Mass in kg} = \frac{K \times M \times R \times T}{16}$$

where:

K means:

- (a) 0.055 if a single drive axle is fitted to the motor vehicle, or
- (b) 0.053 if a single drive tandem axle group is fitted to the motor vehicle, or
- (c) 0.051 if a dual drive tandem axle group is fitted to the motor vehicle, and

M means the number of tyre revolutions per kilometre as specified by the tyre manufacturer for the tyres fitted to the driving axle or axles, and

R means the overall gear reduction between engine and drive wheels, and

T means the maximum engine net torque in newton-m.

## Part 2 Size and projection of loads

### 7 Size limits

- (1) A vehicle or a combination, and its load, must not exceed a size limit set for the vehicle or combination in the *Motor Traffic Regulations 1935*.
- (2) The distance measured at right angles between the rear overhang line of a vehicle and the rear of any load it is carrying must not exceed the rear overhang that the vehicle is allowed under the *Motor Traffic Regulations 1935*.

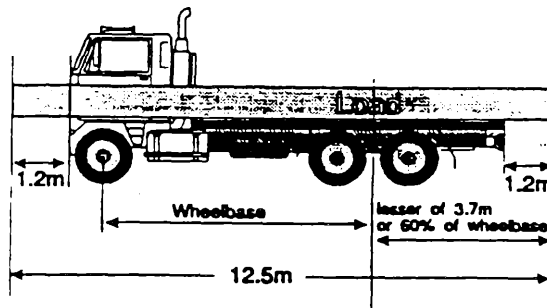
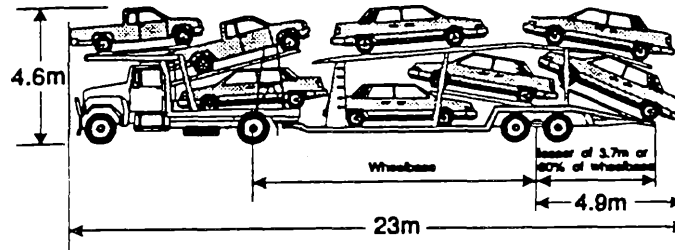


Illustration Of front projection limit; and rear overhang limit from the Motor Traffic Regulations 1935; and maximum rear projection of toad allowed without a warning signal

- (3) Despite subclauses (1) and (2):
  - (a) the height of a vehicle that is carrying vehicles on more than one deck, and its load, must not exceed 4.6 m, and
  - (b) the distance measured at right angles between the rear overhang line of a trailer carrying vehicles on more than one deck and the rear of the rearmost vehicle on the trailer must not exceed 4.9 m.

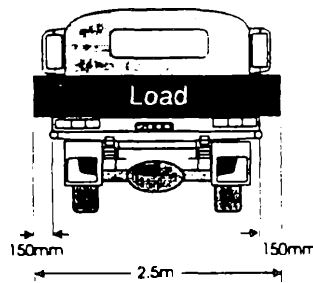




Maximum allowable dimensions of a loaded car carrier

**8 Front and side projections**

A load on a vehicle must not project more than 1.2 m in front of the vehicle, or more than 150 mm from the outermost part of either side of it.



Vehicle loaded to width limits referred to in clause 7 (l), with a maximum side projection of load allowed under clause 8 (1)

**Note.** The combined dimensions of a vehicle and its load must still meet the requirements of clause 7 of this Schedule even if the load projects from the vehicle in any direction.

**9 Rear projections**

- (1) The rear of a load on a vehicle must carry a warning signal if the load:

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- (a) projects more than 1.2 m behind the vehicle, or
  - (b) projects to the rear of the vehicle so that the end of the load cannot be seen easily from behind, or
  - (c) is on a pole-type trailer.
- (2) In daytime, the warning signal must be a brightly coloured flag or piece of material, with each side at least 300 mm long.
- (3) In the night-time, the warning signal must be a red light which can be seen for 200 m.

### **10 Dangerous projections**

A load on a vehicle must not project in a way that is dangerous to a person or to property, even if all dimension and warning requirements are met.

## **Part 3 Couplings**

### **11 Trailers**

- (1) A trailer in a combination must be securely coupled to the vehicle in front of it.
- (2) The components of a coupling used between vehicles must be compatible and properly connected to each other.

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## Schedule 2    Oversize and overmass vehicles

(Clause 9)

### Part 1    All vehicles travelling under notices

#### 1    Mass limits relating to tyre width

- (1) The mass on an axle fitted with tyres of a number and width described in Table 1 must not exceed the mass limit specified in the Table for that axle in relation to the narrowest tyre on the axle.

**Table 1**

Tyre width of the narrowest tyre on the axle (mm)		Mass limit if the axle has 2 tyres (tonnes)	Mass limit if the axle has 4 tyres (tonnes)
<i>at least</i>	<i>but less than</i>		
190	228	4.5	9.0
228	254	5.0	9.5
254	279	6.0	10.0
279	305	6.5	11.0
305	330	7.0	12.0
330	356	7.5	13.0
356	381	8.0	14.0
381	406	9.0	14.0
406	458	10.0	14.0
458	508	11.0	14.0
508		12.0	14.0

- (2) The mass on an axle group fitted with tyres of a number and width described in Table 2 must not exceed the mass limit specified in the Table for that axle group in relation to the narrowest tyre in the group.

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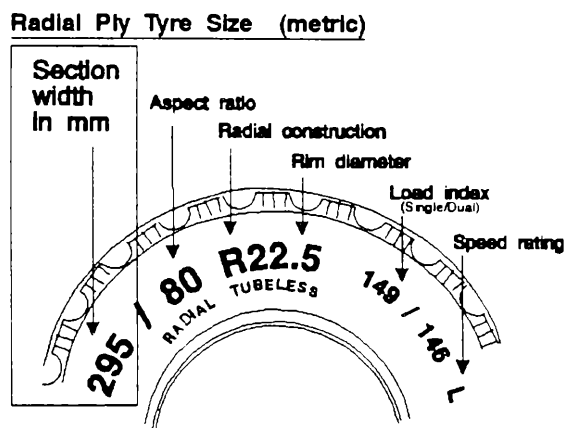
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Schedule 2 Oversize and overmass vehicles

**Table 2**

Tyre width of the narrowest tyre in the group (mm)		Mass limit for a tandem axle group with 4 tyres (tonnes)	Mass limit for a tri-axle group with 6 tyres (tonnes)
at least	but less than		
381	406	16.5	22.0
406	431	17.0	23.0
431	458	17.5	24.0
458	482	18.0	25.0
482	508	18.5	26.0
508		19.0	27.0

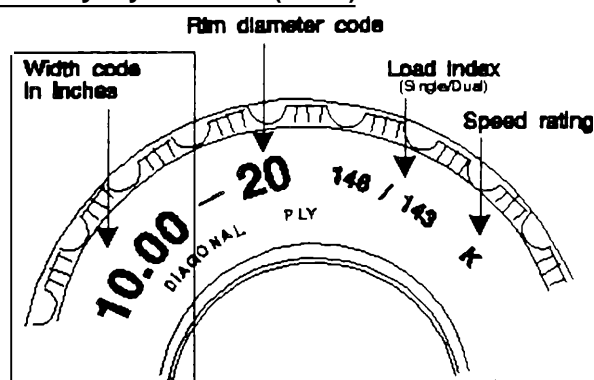
- (3) For the purposes of Tables 1 and 2, the tyre width of a radial ply tyre is the number of millimetres marked on the tyre in the position labelled “Section width in mm.” on the diagram below.



**Position of section width marking on radialply tyre**

- (4) For the purposes of Tables 1 and 2, the tyre width of a bias-type tyre is the number of millimetres equal to 25.4 times the number marked on the tyre in the position labelled “Width code in inches” on the diagram below.

#### Bias Pty Tyre Size (code)



#### Position of width code marking on diagonal or bias type tyre

- (5) If no section width or width code is marked on a tyre, the tyre width for the purposes of Tables 1 and 2 may be determined by measuring the width of the part of the tyre that normally comes into contact with the road surface.

## 2 Dimension limits

- (1) A vehicle or combination described in Table 3 must not, together with any load, exceed a dimension limit specified in the Table for that vehicle or combination, or the corresponding limit in the Motor Traffic Regulations 1935 or Schedule 1, whichever is the greater.

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**Table 3**

Type of vehicle or combination	Dimension	Limit (m)
Any except agricultural	Width	3.5
Any except agricultural	Height	A height specified in the notice, being a height not less than 4.3 and not greater than 4.6
Any combination except agricultural	Length	25.0
Special purpose vehicle	Length	14.5
Other rigid vehicle	Length	12.5
Special purpose vehicle	Distance from the rear overhang line to rear of the vehicle	Lesser of 4 m and 90% of the wheelbase
Semi-trailer	Distance from the rear overhang line to the rear of the vehicle (or load)	Lesser of 5.5 m and 25% of the semi-trailer's length
Special purpose vehicle	Projection in front of the centre of the steering wheel	3.5

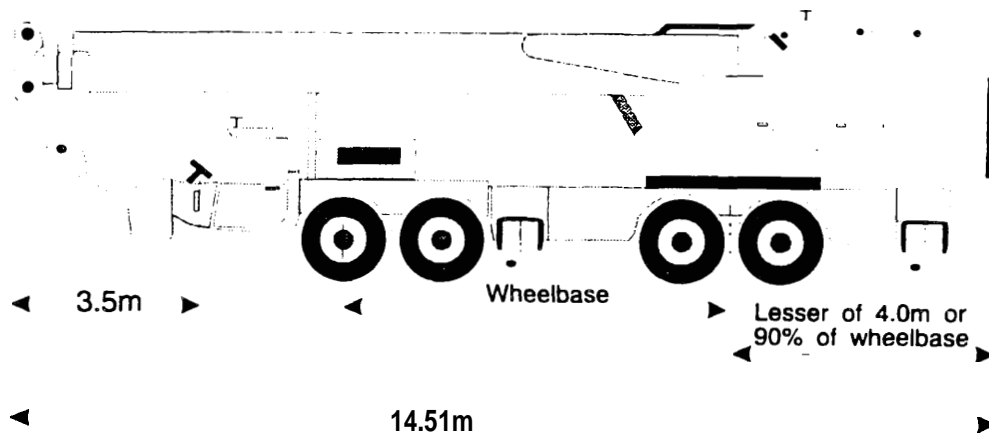
**Note.** Dimension limits for agricultural vehicles are set out in clause 25 of this Schedule.

The combined dimensions of a vehicle and its load must still comply with the requirements of Table 3 even if the load projects from the vehicle in any direction.

**Note.** A notice may specify routes for particular classes of vehicles under clause 11 of this Regulation. For example vehicles under 4.5 m in height may be allowed to travel on a greater number of routes than vehicles 4.6 m high.

- (2) Any flags, lights or mirrors required on a vehicle, combination or load must be disregarded when measuring the dimensions of a vehicle or combination for the purposes of this Regulation.

- (3) The centre lines of adjacent axles in an axle group on an overmass vehicle or combination must be at least 1.2 m apart.



**Rigid mobile crane, showing dimension limits**

### 3 Making an unloaded vehicle smaller

An oversize vehicle without a load must be reduced to the smallest practicable dimensions, with any booms fully retracted.

### 4 Warning devices for oversize vehicles

- (1) An oversize vehicle or Combination, together with any load, that is wider than 2.5 m, or longer than 25 m, must have:
- one warning sign at its front, and
  - one warning sign at its rear or, if it is carrying a rear-projecting load, at the rear of the load, and
  - 4 brightly coloured red, yellow, or red and yellow, flags, each at least 450 mm long and at least 450 mm wide.
- (2) One of the flags must be positioned:
- at each side of the front and rear of any projecting load, or
  - if there is no projecting load, at each side of the front and rear of the vehicle or combination.

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- (3) An oversize vehicle or combination that, together with any load, is not wider than 2.5 m, and whose length is more than 22 m but not more than 25 m, must have one warning sign at its rear or, if it is carrying a rear-projecting load, at the rear of the load.
- (4) Subclauses (1), (2) and (3) do not apply to:
- (a) a road construction vehicle or combination travelling within 1 km of a construction site, if the vehicle has a warning light, or
  - (b) a tractor, or
  - (c) a rigid mobile crane less than 3 m wide.
- (5) If a load projects more than 150 mm beyond one side of an oversize vehicle or combination, and the projection is less than 500 mm thick from top to bottom, there must be:
- (a) a warning light attached to the vehicle or combination, and
  - (b) at least two yellow, rigid pieces of material (in this clause called *delineators*), one attached to the front and the other attached to the rear of the projection.
- Note.** If the load projects more than 150 mm beyond both sides of the vehicle there must be at least 4 delineators—at least one on each side at the front and at least one on each side at the rear.
- (6) A delineator must:
- (a) be at least 300 mm long and at least 300 mm wide, and
  - (b) comply with Class 1 or 2 of Australian Standard AS 1906, “Retro-reflective Materials and Devices for Road Traffic Control Purposes”, 1990 as amended and in force at the commencement of this Regulation, and
  - (c) if at the front of the projection, be attached so that its reflective surface is facing forward of the vehicle, and
  - (d) if at the rear of the projection, be attached so that its reflective surface is facing rearward from the vehicle.
- (7) In the daytime, an oversize vehicle or combination must display a warning light if the vehicle, together with any load, is wider than 3 m.



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**Notes—Other warning requirements**

As well as complying with subclauses (5), (6) and (7) and clause 6 of this Schedule:

- (a) tractors (and other agricultural vehicles) must comply with the warning requirements in clause 26 of this Schedule, and
- (b) rigid mobile cranes (and other special purpose vehicles) must comply with the warning requirements in clauses 20 and 21 of this Schedule.

Parts 6 and 7 set out requirements for warning lights and signs.

In addition to the requirements in this clause, any load projecting more than 1.2 m behind the vehicle must display warning flags in the daytime, in accordance with Schedule 1.

**5 Additional warning devices at night**

At night, an oversize vehicle or combination must display:

- (a) lights showing yellow to the front and red to the rear (known as *side markers*) no more than 2 m apart along both sides of the vehicle and along any front or rear projection, and
- (b) two red lights (known as *rear markers*) fixed to the rear of any rear-projecting load, within 400 mm of each side of the load, and at least 1 m but not more than 2.1 m above the ground, and
- (c) a warning light if the vehicle or combination, together with any load, is wider than 2.5 m or longer than 22 m.

**Note.** Clause 20 of this Schedule sets out additional lighting requirements for special purpose vehicles travelling at night.

**6 Headlights**

An oversize vehicle, or the front vehicle in an oversize combination, must have its low-beam headlights on while travelling in the daytime, unless it is not required under the *Motor Traffic Regulations 1935* to have headlights.

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### 7 Travel restrictions at night

At night, a vehicle or combination that, together with any load, is wider than 3.1 m or longer than 22 m, must not:

- (a) travel outside an urban area, or
- (b) travel in an urban area without being accompanied by a pilot vehicle.

**Note.** Urban areas are designated by the Authority—see clause 14 of this Regulation. Permits for night travel outside the conditions set out in the clause above are uncommon.

### 8 No travelling if low visibility

- (1) A vehicle or combination must not begin to travel if, due to circumstances such as fog, heavy rain, smoke, dust or insect plague:
  - (a) visibility is less than 250 m in the daytime, or
  - (b) the headlights of a vehicle approaching within 250 m could not be seen at night.
- (2) If a vehicle or combination is already travelling when visibility is reduced to the level described in subclause (1), the driver must drive it into the nearest safe parking area, and wait until visibility improves beyond that level before continuing to travel.

### 9 Minimum following distance

A person driving an oversize vehicle or combination must maintain a distance of at least 200 m from an oversize vehicle or Combination travelling in front of it, unless:

- (a) it is overtaking the front one, or the front one is stopping, or
- (b) there is a separate lane available for the use of overtaking traffic, or
- (c) it is in an urban area and it is not reasonably practicable to maintain a distance of 200 m.

**10 Assessing routes**

- (1) Before a vehicle or combination is driven along any route, its driver and owner must be satisfied that the route has been assessed and that the vehicle or combination can be driven along it without contravening subclause (2).
- (2) A vehicle or combination must not be driven along a route if to do so would be likely to cause:
  - (a) disruption to telecommunication, electricity, rail, gas, water or sewage services, or
  - (b) damage to a road (including a bridge), structure, rail crossing or tree.
- (3) Subclause (2) does not apply if the authority responsible for the services or property has given permission for the vehicle or combination to travel along the route, and the vehicle or combination is driven in accordance with the permission.

**Note.** This clause prohibits vehicles and combinations larger or heavier than a limit shown on a sign relating to a road or bridge from using the road or bridge.

**Part 2 Load-carrying vehicles travelling under notices****11 Application of Part**

This Part applies only to load-carrying vehicles and combinations.

**12 Mass limits for axles and axle groups**

- (1) The mass on a single axle or axle group described in Table 4 must not exceed the mass limit specified opposite its description in the Table if it is in a Combination consisting of a tandem drive prime mover towing:
  - (a) a low loader, or

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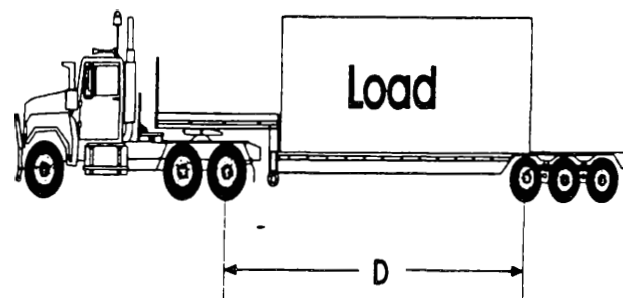
- (b) a low loader dolly and a low loader, or
- (c) a jinker, or
- (d) a low loader dolly and a jinker.

**Table 4**

<b>Single axle or axle group</b>	<b>Mass limit (tonnes)</b>
Steer axle with 2 tyres	6.0
Twinsteer axle group without a load-sharing suspension system	10.0
Twinsteer axle group with a load-sharing suspension system	11.0
Single axle with 8 tyres	12.0
Tandem axle group with 8 or more tyres, not on a trailer	18.5
Tandem axle group with 8 or more tyres, on a trailer	18.5
Tandem axle group with 16 or more tyres, on a trailer	21.0
Tri-axle group with 12 or more tyres	25.0
Oversize tri-axle group with 12 or more tyres or quad axle group with 16 or more tyres	27.0

- (2) The sum of the mass on each of the combination's single axles and axle groups must not exceed the sum of the mass limits specified for each of them in Table 4.
- (3) In addition, the total mass of the combination and any load must not exceed:
  - (a) 49.5 tonnes if the distance between the centre line of the rearmost axle of the towing vehicle and the centre line of the foremost axle of the combination's rear axle group is 6 m or more, or

- (b) 49.5 tonnes decreased by 1 tonne for every 0.3 m by which the distance referred to in paragraph (a) is less than 6 m.



- (4) The mass on an axle group or single axle must not exceed the relevant limit set by this Regulation if it is:
- not described in Table 4, or
  - on a load-carrying vehicle or combination except one described in subclause (1).

### 13 Minimising width

- If a load can be safely loaded in more than one way, it must be loaded in a way that minimises the width of the vehicle or combination and its load.
- An unladen low loader, low loader dolly or jinker, with 4 tyres on each axle, must not be wider than 2.5 m.
- An unladen low loader, low loader dolly or jinker, with 8 tyres on each axle, must not be wider than 2.7 m.

### 14 Carrying goods in addition to a large indivisible item

- A vehicle or combination must not carry more than one large indivisible item unless:
  - the vehicle or combination and its load complies with the mass limits in Schedule 1, and

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- (b) the carrying of additional large indivisible items does not cause the vehicle or combination and its load to exceed a dimension limit in Schedule 1 that would not have been exceeded by the carrying of one of the large indivisible items.
- (2) A vehicle or combination carrying one or more large indivisible items must not carry any other goods unless:
  - (a) the vehicle or combination and its load complies with the mass limits in Schedule 1, and
  - (b) the other goods are contained within the dimension limits in Schedule 1.
- (3) Despite subclause (2), a vehicle or combination carrying a special purpose vehicle or agricultural vehicle may also carry any equipment, tools, substances or detached parts to be used in conjunction with the vehicle being carried.

### 15 Towing a low loader dolly with an unladen low loader

An unladen low loader must not be towed in a combination with a low loader dolly unless:

- (a) the combination is 2.5 m wide or less, and
- (b) it would be unreasonable to require the dolly to be loaded onto the low loader because of the short distance to be travelled, or special difficulties in loading or unloading the dolly due to the nature of the site.

## Part 3      Special purpose vehicles travelling under notices

### 16 Application of Part

This Part applies only to special purpose vehicles.

**17 Mass limits for axles and axle groups**

- (1) The mass on a single axle or axle group described in Table 5 must not exceed the mass limit specified opposite its description in the Table.

**Table 5**

Type of Axle or Axle group	Mass limit (tonnes)
Single axle or single axle group fitted with:	
(a) single tyres	7
(b) dual tyres	10
Twinsteer axle group:	
(a) without a load-sharing suspension system	10
(b) with a load-sharing suspension system	14
Tandem axle group fitted with single tyres	14
Tri-axle group fitted with single tyres	18

- (2) The mass on a tandem axle group fitted with dual tyres, where the centre lines of the axles are less than 1.35 m apart, must not exceed 20 tonnes:
- (a) decreased by 1 tonne for each 100 mm by which the axle group's ground contact width is less than 2.4 m, or
- (b) increased by 1 tonne for each 100 mm by which the axle group's ground contact width exceeds 2.5 m, but not increased to more than 27 tonnes.
- (3) The mass on a tandem axle group fitted with dual tyres, where the centre lines of the axles are at least 1.35 m apart, must not exceed 23 tonnes:
- (a) decreased by 1 tonne for each 100 mm by which the axle group's ground contact width is less than 2.4 m, or
- (b) increased by 1 tonne for each 100 mm by which the axle group's ground contact width exceeds 2.5 m, but not increased to more than 27 tonnes.

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- (4) The mass on a tri-axle group fitted with dual tyres, where the centre lines of the axles are less than 1.35 m apart, must not exceed 25 tonnes, decreased by 1 tonne for each 100 mm by which the axle group's ground contact width is less than 2.4 m.

**Note.** Under clause 2 (3) of this Schedule the centre lines of the axles must be at least 1.2 metres apart.

- (5) The mass on a tri-axle group fitted with dual tyres, where the centre lines of the axles are at least 1.35 m apart, must not exceed 27 tonnes, decreased by 1 tonne for each 100 mm by which the axle group's ground contact width is less than 2.4 m.

### 18 Total mass limits for special purpose vehicles

The total mass of a special purpose vehicle must not exceed the least of

- (a) the sum of the mass allowed for each single axle and axle group on the vehicle, and
- (b) 40 tonnes, and
- (c) the mass worked out using the following formula:

$$\text{Mass in tonnes} = 3L + 15 \pm G$$

where:

*L* is the distance in metres between the centre lines of the vehicle's foremost and rearmost axles, and

$\pm G$  is a number of tonnes:

- (i) to be added at the rate of 1 tonne for each 100 mm by which the ground contact width of the rearmost axle exceeds 2.5 m, or
- (ii) to be subtracted at the rate of 1 tonne for each 100 mm by which the ground contact width of the rearmost axle is less than 2.4 m.



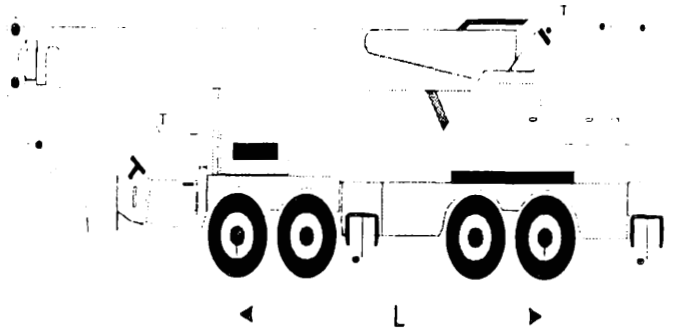


illustration of distance "L"

### 19 Side lights for travel at night

- (1) When travelling at night, a special purpose vehicle with a projection extending more than 1.2 m in front of the vehicle body must have a yellow light fixed on each side of the projection, mounted as far forward as possible, and shielded from the driver's view.
- (2) Both lights must be visible to any traffic approaching the vehicle from its front, and at least one of the lights must be visible to any traffic approaching the vehicle from either side.

### 20 Markings for special purpose vehicles

- (1) Rear marker plates complying with ADR 13/00, must be displayed at the rearmost part of the body of a special purpose vehicle.
- (2) A pattern covering an area one of at least 0.16 square metres and consisting of diagonal stripes at least 150 mm wide and alternately coloured:
  - (a) red and white, or
  - (b) black and white,

must be displayed on the left and right sides of any rigid projection extending more than 1.2 m in front of the body of a special purpose vehicle, if it is practicable to do so.

## 21 Prohibition on towing other vehicles

A special purpose vehicle must not tow a vehicle.

## Part 4      Agricultural vehicles travelling under notices

### 22 Application of Part

This Part applies only to agricultural vehicles and agricultural combinations.

### 23 Mass limits

The mass on an axle described in Table 6, on an agricultural machine or implement with 2 axles, must not exceed the mass limit specified opposite its description in the Table.

**Table 6**

<b>Number of tyres on an axle</b>	<b>Mass limit (tonnes)</b>
Axle fitted with 2 tyres	9.0
Axle fitted with 4 tyres and a ground contact width less than 2.51 m	9.0
Axle fitted with 4 tyres and a ground contact width at least 2.51 m but not more than 3.5 m	10.5
Axle fitted with 4 tyres and a ground contact width more than 3.5 m	12.0

### 24 Dimension limits

- (1) The height of an agricultural vehicle must not exceed 4.6 m.
- (2) On an agricultural machine or implement, the distance from the rear overhang line to the rear of the vehicle must not exceed 4.5 m (except for augers or conveyors, on which the distance must not exceed 5.5 m).
- (3) The length of an agricultural machine must not exceed 12.5 m.
- (4) The length of an agricultural combination must not exceed 25 m.

- (5) The width of an agricultural machine or combination described in Table 7, in an area determined by the Authority to be in a category specified in Table 7, must not exceed the limit specified opposite that category in the Table.

**Table 7**

Category of Area	Width limit for an agricultural machine alone or an agricultural machine or rigid vehicle towing an agricultural implement
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metres

Category 1	3.5
Category 2	5.0
Category 3	6.0

- (6) An agricultural vehicle or combination of a type or in an area not covered by Table 7 must not be wider than 2.5 m.

**Note.** An agricultural vehicle or combination not covered by Table 7 must get a permit in order to travel at a greater width than 2.5 m.

## 25 Warning devices for agricultural vehicles

- (1) An agricultural vehicle or combination that is wider than 2.5 m, or an agricultural combination that is longer than 22 m, must have a warning light fixed to its highest practicable point when it is on a major road.
- (2) A warning sign must be displayed at the rear of an agricultural implement being towed by a tractor.

**Note.** Under clause 4 of this Schedule, single tractors do not have to be fitted with warning signs.

- (3) A pattern covering an area of at least 0.16 square metres and consisting of diagonal stripes at least 150 mm wide and alternately coloured:
- red and white, or
  - black and white,

must be displayed at the sides of the rear of any auger, conveyor, or harvester comb carried on a comb trailer.

- (4) Any part of an axle extending more than 150 mm from the outside wall of a tractor's tyre must be painted fluorescent yellow or have yellow fluorescent or other high-visibility material wrapped around it.

**Note.** See clauses 4 and 5 of this Schedule for additional warning requirements for agricultural vehicles.

**26 Pilot vehicle requirements for agricultural vehicles**

- (1) Agricultural vehicles and combinations of a width described in Table 8 must be accompanied by pilot vehicles in accordance with the Table.

**Table 8**

Area of operation	Wider than 3.7 m but not wider than 4.5 m	Wider than 4.5 m but not wider than 6 m
If the terrain limits a clear view of approaching traffic to less than 500 m	1 pilot vehicle	2 pilot vehicles
A major road	1 pilot vehicle	2 pilot vehicles
If there is a clear view of approaching traffic for at least 500 m, but not on a major road	nil	1 pilot vehicle

- (2) Despite subclause (1), an agricultural vehicle or combination does not have to be accompanied by a pilot vehicle if it is travelling less than 500 m.

**27 Agricultural vehicles not to use freeways**

An oversize agricultural vehicle or combination must not be driven on a freeway.

**28 Agricultural implements not to carry loads**

An agricultural implement must not carry a load.

**29 Speed limits for agricultural implements without brakes**

An agricultural implement without brakes must not be towed at a speed greater than the lesser of:

- (a) 20 km an hour less than the speed limit applying to the section of road on which the implement is travelling, and
- (b) 50 km an hour.

**30 Exemption from requirement for rear vision mirrors**

A vehicle towing an agricultural implement wider than 3.5 m, which obscures the view to the rear of the vehicle, is exempt from any requirement under the *Motor Traffic Regulations 1935* to have a rear vision mirror fitted.

**Note.** Regulation 123AB of the *Motor Traffic Regulations 1935* provides that a person or vehicle is exempted from a dimension limit or any other requirement of those Regulations if that person or vehicle is exempted from that limit or requirement under this Regulation.

**Part 5 Pilot and escort vehicles****31 Application of Part**

This Part applies to the operation of a vehicle as a pilot vehicle or escort vehicle that is required by a notice or permit under this Regulation.

**32 Requirements for a pilot vehicle**

- (1) A pilot vehicle must have 4 or more wheels and a GVM of:
  - (a) 6.5 tonnes or less in the case of a rear pilot vehicle if two pilot vehicles are required, or
  - (b) 4.5 tonnes or less in any other case.
- (2) A pilot vehicle must have a warning sign on its roof.
- (3) A pilot vehicle must only have a warning light attached:
  - (a) above or below the sign, or
  - (b) at each side of the sign.

**Note.** Parts 6 and 7 of this Schedule set out requirements for warning lights and signs.

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### 33 Requirements for an escort vehicle

- (1) An escort vehicle other than a police motor cycle must have:
  - (a) 4 or more wheels, and
  - (b) a GVM of 4.5 tonnes or less, and
  - (c) on its roof a light that flashes or 2 lights that flash when switched on.
- (2) A light referred to in subclause (1) must flash yellow unless the vehicle is a police vehicle or a vehicle belonging to an Authority and the light is of a colour approved under the *Motor Traffic Regulations 1935*.

### 34 Headlights on a pilot or escort vehicle

The low-beam headlights on a pilot vehicle or escort vehicle must be switched on when it is accompanying an oversize vehicle or combination during the daytime.

### 35 What may a pilot or escort vehicle carry?

A pilot or escort vehicle must not tow a trailer or carry a load, but it may carry tools, equipment or substances for use in connection with the oversize vehicle or combination that it is accompanying or for restraining the load on that vehicle or combination.

### 36 Where must a pilot vehicle be driven?

- (1) When 1 pilot vehicle accompanies an oversize vehicle or combination, the pilot vehicle must travel:
  - (a) behind the oversize vehicle or combination if they are on a divided road, or
  - (b) in front of the oversize vehicle or combination if they are on a road that is not divided.
- (2) When 2 pilot vehicles accompany an oversize vehicle or combination, one pilot vehicle must travel in front of the oversize vehicle or combination, and the other behind it.

- (3) A pilot vehicle must travel far enough away from the oversize vehicle or combination it is accompanying to give adequate warning to other road users of the presence of the oversize vehicle or combination, taking into account traffic speed, weather, visibility and other driving conditions.

### **37 Communication between drivers**

- (1) An oversize vehicle or combination and any accompanying pilot or escort vehicle must have an electronic device that allows the drivers to communicate effectively with each other.
- (2) Subclause (1) does not apply to:
- (a) an oversize agricultural machine, or
  - (b) an oversize combination that includes an agricultural machine, or
  - (c) an escort vehicle that is a police vehicle.

## **Part 6 Warning lights**

### **38 Characteristics of warning lights**

When switched on, a warning light on an oversize vehicle or combination or pilot vehicle must:

- (a) emit a rotating, flashing, yellow coloured light, and
- (b) flash between 120 and 200 times a minute, and
- (c) have a power of at least 55 watts, and
- (d) not be a strobe light.

### **39 Visibility of warning lights**

- (1) A warning light on an oversize vehicle or combination or a pilot or escort vehicle must be:
- (a) clearly visible at a distance of 500 m in all directions, or
  - (b) supplemented by one or more additional warning lights so that the light emanating from at least one of them is clearly visible at a distance of 500 m in any direction.

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- (2) Despite subclause (1), in the case of a pilot vehicle travelling in front of an oversize vehicle or combination, a filter may be placed behind the warning light on the pilot vehicle, to reduce the intensity of the light directed to the driver of the oversize vehicle or combination.

#### **40 Warning lights to be on if required, and off if not**

- (1) Any warning light which an oversize vehicle or combination is required to have must only be switched on when the vehicle or combination is travelling or is stationary in a position that is likely to cause danger to other road users.
- (2) Any warning light which a pilot or escort vehicle is required to have must be switched on when the vehicle is travelling and accompanying an oversize vehicle or combination.
- (3) If a vehicle or combination is not required under this Regulation to have a warning light, it must not have one that is switched on.

## **Part 7      Warning signs**

### **Division 1      When this Part applies**

#### **41 What specifications must a warning sign comply with?**

A warning sign must comply with the specifications in this Part:

- (a) if it is on an oversize agricultural vehicle or combination,  
or
- (b) if it is on another type of oversize vehicle or combination,  
or a pilot vehicle.

### **Division 2      Oversize vehicles and pilot vehicles**

#### **42 Face of a warning sign**

- (1) The face of a warning sign must have a yellow surface that complies with Class 1 or 2 of Australian Standard AS 1906, "Retro-reflective Materials and Devices for Road Traffic Control Purposes", as amended and in force at the commencement of this Regulation, Parts 1 to 4 (inclusive).



- 
- (2) The face of the warning sign must have a black border at least 20 mm wide.
  - (3) The outermost edge of the border must be set at least 10 mm in from the edge of the sign unless the sign has been made with a box edge.
  - (4) The warning sign must have its manufacturer's name or trademark permanently marked in letters at least 3 mm but not more than 10 mm high.
  - (5) The marking may appear in any visible location on the sign, except in a bottom corner of a sign used on a pilot vehicle.

#### **43 Material for a warning sign**

- (1) A warning sign must be made of stiff, flat, weatherproof material.

**Note.** Zinalume at least 0.8 mm thick, and aluminium at least 1.6 mm thick are examples of material that would comply with subclause (1).

- (2) Despite subclause (1), the rem sign on an oversize vehicle or combination may be made of flexible material if the load is unsuitable for a stiff sign.

#### **44 Keeping signs clean**

A warning sign on a vehicle must be kept clean enough so that it can be easily read by other road users.

#### **45 Warning sign must not be displayed if not required**

A warning sign must not be displayed on a vehicle or combination that is not operating oversize or as a pilot vehicle.

### **Division 3 Oversize vehicles only**

#### **46 Size of a warning sign**

- (1) A warning sign on an oversize vehicle or combination must be at least 1200 mm long and at least 450 mm high.

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- (2) The sign may be split into two parts, in which case the combined length of its parts must be at least 1200 mm.

### 47 Face of a warning sign

- (1) A warning sign on an oversize vehicle or combination must show the word “OVERSIZE”, in black upper-case lettering, conforming with Australian Standard AS 1744, “Forms of Letters and Numerals for Road Signs”, in typeface Series C (N) as amended and in force at the commencement of this Regulation.
- (2) The lettering must be at least 200 mm high.
- (3) The top and the bottom of the lettering must be at least 125 mm from the top and bottom of the sign, respectively.
- (4) If the sign is split into two parts:
- (a) the part mounted on the left must show the letters “OVER”, and the part mounted on the right must show the letters “SIZE”, and
  - (b) there must be no border between the two parts, despite subclauses (2) and (3).



illustration of a warning sign for an oversize vehicle or  
combination

### 48 Mounting a warning sign

- (1) A warning sign on an oversize vehicle or combination must be mounted vertically.
- (2) The lower edge of the sign must be:
- (a) above the bottom of the bumper bar, or

- 
- (b) if there is no bumper bar—at least 500 mm from the ground level.
  - (3) If the sign is split into two parts, each part must be fitted at the same height as the other.

#### **Division 4 Pilot vehicles only**

##### **49 Size and shape of a warning sign**

- (1) A warning sign on a pilot vehicle must be at least 1200 mm long and at least 600 mm high.
- (2) The sign may have bottom corner cut-outs not more than 150 mm wide and not more than 100 mm high if they are needed for mounting the warning lights.

##### **50 Faces of a warning sign**

- (1) Both faces of a warning sign on a pilot vehicle must show:
  - (a) the word “OVERSIZE”, in black upper-case lettering at least 200 mm high, conforming with Australian Standard AS 1744, “Forms of Letters and Numerals for Road Signs”, in typeface Series C(N) as amended and in force at the commencement of this Regulation, and
  - (b) the words “LOAD AHEAD”, in black upper-case lettering at least 100 mm high, conforming with Australian Standard AS 1744, “Forms of Letters and Numerals for Road Signs”, in typeface Series D(N) as amended and in force at the commencement of this Regulation.
- (2) The bottom of the lettering of the word “OVERSIZE” must be at least 300 mm from the bottom of the sign.
- (3) The bottom of the lettering of the words “LOAD AHEAD” must be at least 100 mm from the bottom of the sign.

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Schedule 2      Oversize and overmass vehicles

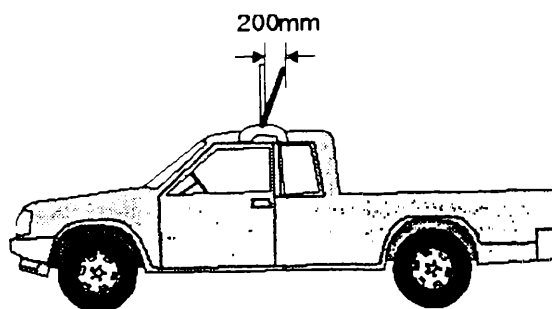
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**Illustration of a warning sign for a pilot vehicle**

**51 Mounting a warning sign**

A warning sign on a pilot vehicle must not lean back so that there is more than 200 mm measured horizontally from the top of the sign to a vertical line running through the bottom of the sign.



**Illustration of maximum slant of warning sign**

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## **Part 8 Interpretation**

### **52 References to vehicles and combinations**

In this Schedule:

- (a) a reference to a vehicle or combination is a reference to a vehicle or combination that is oversize or overmass or both, and
- (b) a reference to a oversize vehicle or combination is a reference to a vehicle or combination that is oversize or oversize and overmass, and
- (c) a reference to an overmass vehicle or combination is a reference to a vehicle or combination that is overmass, or overmass and oversize.

### **53 Measuring distances between parallel lines**

In this Schedule, a reference to a distance between 2 lines that are parallel means the distance measured at right angles between the lines.

### **54 Application to retractable axles**

For the purposes of this Schedule, a retractable axle must be taken to be an axle when it is in the lowered position and must be taken not to be an axle when it is in the raised position.

### **55 Application of mass limits where more than one applies**

If 2 or more mass limits apply to the same axle or axle group as a result of the application of provisions in Table 1, 2, 4, 5 or 6, or in any 2 or more of those tables, the axle or axle group must comply with the lower or lowest of those limits.

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Schedule 3 Load limit notices

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**Schedule 3 Load limit notices**

(Clause 40)



Diagram 1

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## Dictionary

**added load** means the moveable load carried by any vehicle.

**ADR** (Australian Design Rule) means a national standard under the *Motor Vehicle Standards Act 1989* of the Commonwealth.

**agricultural combination** means a combination that includes at least one agricultural vehicle.

**agricultural implement** means a vehicle without its own motive power, built to perform agricultural tasks.

**agricultural machine** means a machine with its own motive power, built to perform agricultural tasks.

**Note.** Examples of agricultural implements are irrigating equipment, augers, conveyors, empty field bins, harvester fronts, and machinery fully carried on the three-point linkage of a tractor. Examples of agricultural machines are tractors and harvesters.

**agricultural vehicle** means an agricultural implement or agricultural machine.

**approved air suspension system**, in relation to a vehicle, means a suspension system in which:

- (a) vertical movement between each axle and the body of the vehicle is controlled by variations in the pressure of air in an airspring, and
- (b) the proportion of the vehicle's mass that is borne by the spring remains substantially constant despite variations in the pressure of air in the airspring.

**at night** means during the period between sunset and sunrise.

**Australian Standard** means a standard, approved for publication on behalf of the Council of the Standards Association of Australia, as in force at the commencement of the provision in this Regulation in which the expression appears.

**Australian/New Zealand Standard** means a a joint standard, issued or approved for publication on behalf of Standards Australia and Standards New Zealand under a mutual recognition agreement called "Active Cooperation between Australia and New Zealand for the Preparation and Marketing of Joint Standards", as is in force at the commencement of the provision in this Regulation in which the expression appears.

**Authority** means:

- (a) the Minister, or

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#### Dictionary

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- (b) a person to whom the powers of the Minister are delegated for the purposes of the provision in which the term is used.

**axle** means one or more shafts positioned in a line across a vehicle, on which one or more wheels intended to support the vehicle turn.

**axle group** means a single axle group, tandem axle group, twinsteer axle group, tri-axle group or quad-axle group.

**axle load** means the total load, determined in accordance with this Regulation, transmitted to the road by all wheels of a vehicle which are mounted on any axle.

**B-double** means a combination consisting of a prime mover towing 2 semi-trailers.



Typical B-double

**bridge** includes any gate, pier, fender, dolphin or platform or any other thing incidental to the use or protection of the bridge.

**bus** means a motor vehicle currently principally fitted or equipped to carry more than 9 seated adults persons (including the driver).

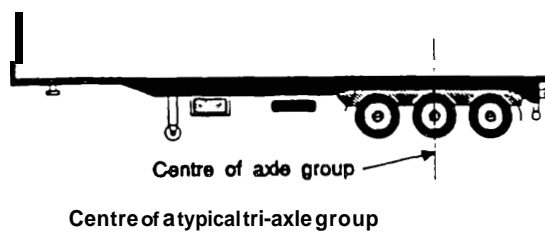
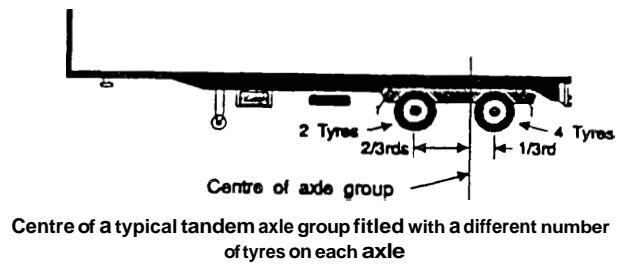
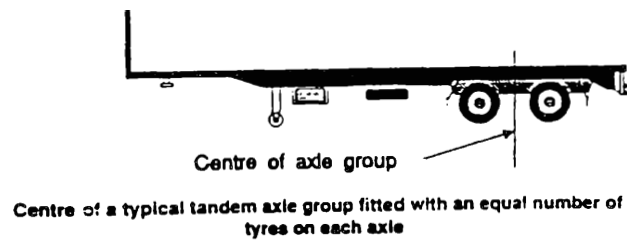
**centre line**, in relation to an axle, means:

- (a) in the case of an axle consisting of one shaft—a line parallel to the length of the axle and passing through its centre, and
- (b) in the case of an axle consisting of 2 shafts—a line that is in the vertical plane passing through the centre of both shafts and that passes through the centres of the wheels on the shafts.

**centre of an axle group** means:

- (a) a line located midway between the centre-lines of the outermost axles of the group, or
- (b) if the group consists of 2 axles, one of which is fitted with twice the number of tyres as the other axle—a line located one third of the way from the centre-line of the axle with more tyres towards the centre-line of the axle with fewer tyres.



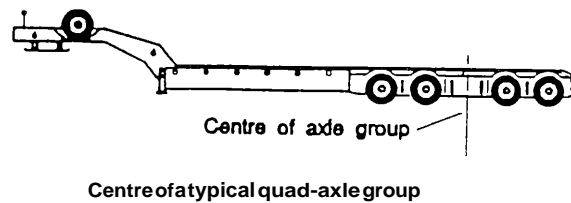


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**checking station** means an area indicated by a notice referred to in clause 42 or by an authorised officer exercising functions under that clause.

**Class 1 notice** means a notice published under clause 10 in respect of a vehicle or combination of vehicles.

**Class 1 permit** means a permit issued under clause 12 in respect of a vehicle or combination of vehicles.

**Class 1 vehicle** means a restricted access vehicle that is:

- (a) a special purpose vehicle, or
- (b) an agricultural machine or agricultural implement, or
- (c) a vehicle or combination, including a low loader or load platform combination, that is specially designed for the carriage of a large indivisible item or is carrying a large indivisible item,

that, together with any load, exceeds:

- (d) a mass limit in Schedule 1, or
- (e) a dimension limit in Schedule 1 or the *Motor Traffic Regulations 1935*.

**Class 2 notice** means a notice published under clause 17 in respect of a Class 2 vehicle.

**Class 2 permit** means a permit issued under clause 19 in respect of a Class 2 vehicle.

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**Class 2 vehicle** means a restricted access vehicle that complies with the mass and dimension limits prescribed in the *Motor Traffic Regulations 1935* and, except as provided in paragraph (b) of the definition of **restricted access vehicle**, Schedule 1 and is:

- (a) a B-double, or
- (b) a road train, or
- (c) a controlled access bus not more than 14.5 m long, or
- (d) a combination carrying vehicles on more than one deck that, together with its load, meets one or both of the following criteria:
  - (i) its height exceeds 4.3 m but does not exceed 4.6 m,
  - (ii) its length exceeds 19 m, or
- (e) a single motor vehicle, or a combination, that exceeds 4.3 m, but does not exceed 4.6 m, in height and is built to carry cattle, sheep, pigs or horses.

**Class 3 notice** means a notice published under clause 23 in respect of a Class 3 vehicle.

**Class 3 permit** means a permit issued under clause 25 in respect of a Class 3 vehicle.

**Class 3 vehicle** means a restricted access vehicle other than a Class 1 vehicle or a Class 2 vehicle.

**combination** means a motor vehicle connected to one or more vehicles.

**complying bus** means:

- (a) a bus that:
  - (i) is fitted with a compliance plate in accordance with the *Motor Vehicle Standards Act 1989* of the Commonwealth, indicating that the bus was manufactured on or after 1 July 1994, and
  - (ii) meets the emergency exit specifications in ADR 44, and
  - (iii) meets the rollover strength specifications in ADR 59, and
  - (iv) meets the occupation protection specifications in ADR 68, and
  - (v) is equipped with an approved air suspension system, or

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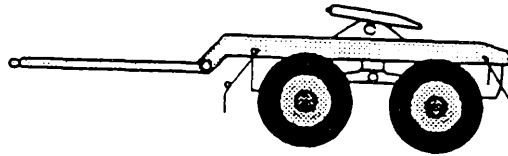
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- (b) a bus that is the subject of a declaration under clause 8 or under a corresponding law of another State or Territory.

**controlled access bus** means a bus that is more than 12.5 m long except a bus that is:

- (a) an articulated vehicle with the meaning of Regulation 2 (1) of the *Motor Traffic Regulations 1935*, or
- (b) an articulated bus as defined in paragraph 10.8 (1) of Part 10 of the appendix to Division 7 of Schedule F to those Regulations.

**converter dolly** means a trailer with one axle group or single axle and a fifth wheel coupling designed to convert a semi-trailer into a dog trailer.



**Typical converter dolly**

**corresponding Authority** in relation to a particular Authority, means an Authority in another jurisdiction whose functions most nearly correspond to those of the particular Authority.

**daytime** means the period beginning at sunrise and ending at sunset.

**dimension limit** includes any limit under this Regulation that applies to a load or projection of any vehicle or combination.

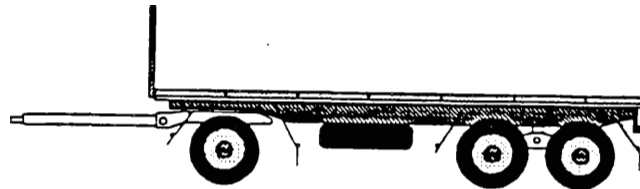
**divisible load** means a load that is:

- (a) made up of more than one indivisible item, or
- (b) a substance or commodity the amount of which can be reduced in size or mass without extreme effort, expense or risk of damage.

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***dog trailer*** means a trailer (including a trailer consisting of a semi-trailer and converter dolly) with:

- (a) one axle group or single axle at the front that is steered by connection to the towing vehicle by a drawbar, and
- (b) one axle group or single axle at the rear.



**Typical dog trailer**

***drawbar*** means a part of a trailer (other than a semi-trailer) that connects the trailer body to a coupling for towing purposes.

***drive*** includes to stand or allow or cause to stand.

***driver*** means the person driving or in control of a motor vehicle.

***escort vehicle*** means a vehicle that is being used:

- (a) to transport a police officer, or other person directing traffic, and
- (b) to warn other road users of the presence of an oversize vehicle or combination.

***extreme axles*** means the two axles, one of which is nearest to the front of the vehicle or combination, as the case may be, and the other of which is nearest to the rear of the vehicle or combination.

***fifth wheel coupling*** means a device, other than the upper rotating element and the kingpin (which are parts of a semi-trailer), used with a prime mover, semi-trailer or a converter dolly to permit quick coupling and uncoupling and to provide for articulation,

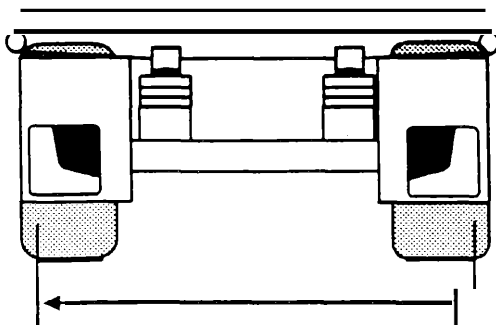
**freight container** means a box-like goods receptacle provided with corner fittings of, or similar to, the types specified respectively in Australian/New Zealand Standard 3711.3 “Freight Containers Corner Fitting”.

**GCM** (gross combination mass), in relation to a motor vehicle, means the greatest possible sum of the maximum loaded mass of the motor vehicle and of any vehicles that may lawfully be towed by it at one time:

- (a) as specified by the motor vehicle’s manufacturer:
  - (i) on a plate fixed to the vehicle by the manufacturer, or
  - (ii) if the manufacturer has not specified the sum of the maximum loaded mass on a plate fixed to the vehicle—in another place, or
- (b) as specified by the vehicle registration authority if:
  - (i) the manufacturer has not specified the sum of the maximum loaded mass, or
  - (ii) the manufacturer cannot be identified, or
  - (iii) the vehicle has been modified to the extent that the manufacturer’s specification is no longer appropriate.

**ground contact width:**

- (a) in relation to an axle, means the distance between the outermost point of ground contact of the outside tyres on each end of the axle, and
- (b) in relation to an axle group, means the greatest ground contact width of all the axles in the group.



**Illustration of ground contact width of an axle**

**GVM** (gross vehicle mass) means the maximum loaded mass of a vehicle:

- (a) as specified by the manufacturer, or
- (b) as specified by the vehicle registration authority if
  - (i) the manufacturer has not specified a maximum loaded mass, or
  - (ii) the manufacturer cannot be identified, or
  - (iii) the vehicle has been modified to the extent that the manufacturer's specification is no longer appropriate.

**hauling unit** means a motor vehicle which forms part of a combination.

**indivisible item** means an item that cannot be divided without extreme effort, expense or risk of damage to it.

**information sheet** means advice published or adopted by the Authority (and available on request from the Authority).

**jinker** means an axle or axle group which is built to support part of a load, and is connected to the vehicle in front of it by a pole or cable or the load itself, if any.

**large indivisible item** means an item that:

- (a) cannot be divided without extreme effort, expense or risk of damage to it, and
- (b) cannot be carried on any vehicle or combination without exceeding a mass or dimension limit in Schedule 1.

**load**, in relation to a vehicle or combination, includes anything that is normally removed from the vehicle or combination when not in use, but, if the vehicle or combination is not constructed to carry anything that is not normally removed from that vehicle or combination when not in use, does not include:

- (a) tools, equipment or substances necessary for the vehicle or combination to function, or for any load to be restrained, or
- (b) personal items used by the driver.

**load-carrying**, in relation to a vehicle or combination, means a vehicle or combination that is carrying, or is built to carry, a load.

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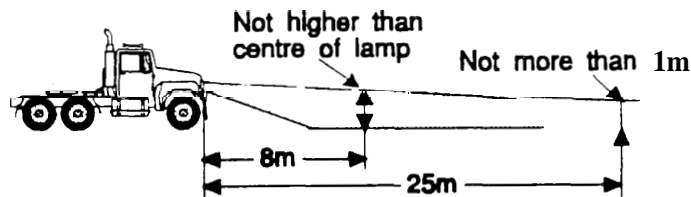
**load-sharing suspension system** means an axle group suspension system that:

- (a) is built to divide the load between the tyres on the group so that no tyre carries a mass more than 10% greater than the mass it would carry if the load were divided equally, and
- (b) has effective damping characteristics on all axles of the group.

**loaded mass** means the mass, determined in accordance with clause 45, of any vehicle and its added load.

**low beam** in relation to a headlight or front fog light fitted to a vehicle, means built or adjusted so that, when the vehicle is standing on level ground, the top of the main beam of light projected is:

- (a) not higher than the centre of the headlight or fog light, when measured at a point 8 m in front of the vehicle, and
- (b) not more than 1 metre higher than the level on which the motor vehicle is standing, when measured at a point 25 m in front of the vehicle.



**low loader** means a gooseneck semi-trailer with a loading deck no more than 1m above the ground.

**low loader dolly** means a mass-distributing device that:

- (a) is usually coupled between a prime mover and a low loader, and
- (b) consists of a gooseneck rigid frame, and
- (c) does not directly carry any goods on itself, and
- (d) is equipped with one or more axles, a king pin and a fifth wheel coupling.

**mass** includes weight.



**Ministerial Council** means the Ministerial Council for Road Transport established by the Heavy Vehicles Agreement, a copy of which is set out in Schedule 1 to the *National Road Transport Commission Act 1991* of the Commonwealth.

**motor vehicle** means a vehicle that is built to be propelled by a motor that forms part of the vehicle.

**multi-modal transport** means the carriage of goods by road and at least one other of the modes sea, rail or air.

**National Road Transport Commission** means the National Road Transport Commission established under the *National Road Transport Commission Act 1991*.

**notice** means a notice issued under this Regulation.

**operate**, in relation to a person who is the owner of a vehicle or combination, includes to allow or cause another person to operate the vehicle or combination.

**overmass** means having a mass that, including the mass of any load, exceeds a relevant mass limit in Schedule 1.

**oversize** means having a dimension that, including the dimension of any load, exceeds a relevant dimension limit in this Regulation or the *Motor Traffic Regulations 1935*.

**oversize tri-axle group** means a group of 3 axles in which the horizontal distance between the centre lines of the outermost axles is more than 3.2 m.

**owner** in relation to a vehicle, means:

- (a) if the vehicle:
  - (i) is registered—the person in whose name the vehicle is registered under a Commonwealth, State or Territory Act, or
  - (ii) is not registered—the person to whom a mark, plate, or permit has been issued to allow the vehicle to be used, and
- (b) the person who, according to the vehicle registration authority's records, has acquired the vehicle from the person in whose name the vehicle is registered under the relevant law, and
- (c) the person who is entitled to the possession of the vehicle.

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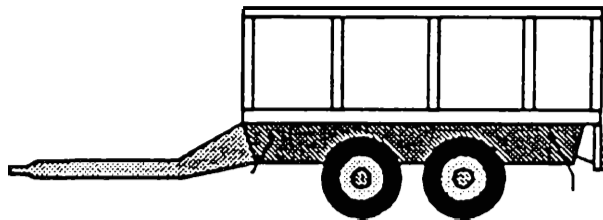
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*permit* means a permit granted under this Regulation.

*pig trailer* means a trailer with one axle group or single axle near the middle of its load-carrying surface, and connected to the towing vehicle by a drawbar.

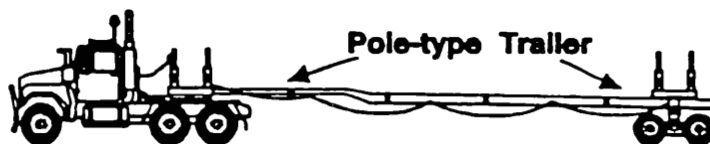


**Typical pig trailer**

*pilot vehicle* means a vehicle being used to warn other road users of the presence of an oversize vehicle or combination.

*pole-type trailer* means a trailer that:

- (a) is attached to a towing vehicle by means of a pole or an attachment fitted to a pole, and
- (b) is ordinarily used for transporting loads, such as logs, pipes, structural members or other long objects, that are generally capable of supporting themselves like beams between supports.



**Typical pole-type trailer**

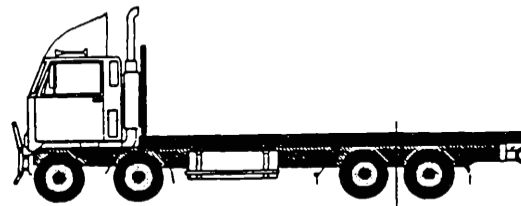
*prime mover* means a motor vehicle built to tow a semi-trailer.

*quad-axle group* means a group of 4 axles, in which the horizontal distance between the centre-lines of the outermost axles is more than 3.2 m but not more than 4.9 m.

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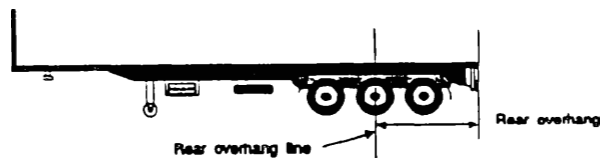
***rear overhang line*** means:

- (a) if there is a single axle at the rear of the vehicle—the centre-line of the axle, or
- (b) if there is an axle group at the rear of the vehicle—the centre of the axle group, determined without regard to the presence of any steerable axle or retractable axle in the group unless all axles in the group are steerable or retractable.



Rear overhang line

**Rear overhang line on a typical motor vehicle that has an axle group**



**Rear overhang line on a typical semi-trailer**

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**restricted access vehicle** means a single motor vehicle or a combination which alone, or together with any load, exceeds one or more of the following limits:

- (a) a mass limit prescribed in this Regulation, or
- (b) one or more of the following dimension limits:
  - (i) a width of 2.5 m,
  - (ii) a height of 4.3 m,
  - (iii) a length of 12.5 m in the case of a single motor vehicle or 19 m in the case of a combination,
  - (iv) any other dimension limit prescribed in this Regulation or the *Motor Traffic Regulations 1935*.

**retractable axle** means an axle that can be raised so that the tyres on the axle do not touch the ground.

**road** has the same meaning as in section 264A of the Act.

**road related area** has the same meaning as in section 264A of the Act.

**road train** means a combination, other than a B-double, consisting of a motor vehicle towing at least 2 trailers (counting as one trailer a converter dolly supporting a semi-trailer).



**Typical triple road train**

**section width of tyre** means the tyre width determined in accordance with clause 1 (3), (4) or (5) of Schedule 2.

**semi-trailer** means a trailer that has:

- (a) one axle group or single axle towards the rear, and
- (b) a means of attachment to a prime mover that would result in some of the load being imposed on the prime mover.

**single axle** means an axle not forming part of an axle group.

**single axle group** means a group of 2 or more axles, in which the horizontal distance between the centre lines of the outermost axles is less than 1 m.

**single motor vehicle** means a motor vehicle that is not part of a combination.

**special purpose vehicle** means a motor vehicle, other than a tow truck or an agricultural vehicle, built for a purpose other than carrying a load, except for water in the case of concrete pumps and fire trucks.

**tandem axle group** means a group of at least 2 axles, in which the horizontal distance between the centre-lines of the outermost axles is at least 1 metre, but not more than 2 m.

**total mass** in relation to a load carrying vehicle or combination means the loaded mass of the vehicle or combination and, in relation to a non load-carrying vehicle or combination, means the mass of that vehicle or combination together with anything that is not usually removed from the vehicle or combination when not in use, other than:

- (a) tools, equipment or substances necessary for the vehicle or combination to function, and
- (b) personal items used by the driver.

**trailer** means a vehicle that is built to be towed, or is towed, by a motor vehicle, but does not include a motor vehicle that is being towed.

**tri-axle group** means a group of at least 3 axles, in which the horizontal distance between the centre-lines of the outermost axles is more than 2 m, but not more than 3.2 m.

**winsteer axle group** means a group of 2 axles:

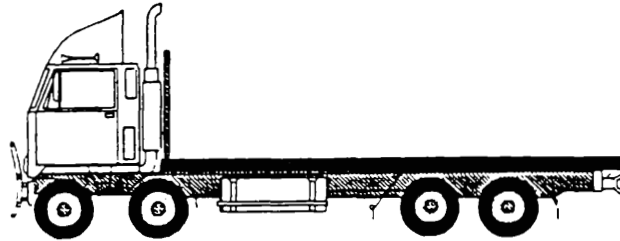
- (a) with single tyres, and
- (b) fitted to a motor vehicle, and
- (c) connected to the same steering mechanism, and
- (d) the horizontal distance between whose centre-lines is at least 1 metre, but not more than 2 m.

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**Typical twinsteer axle group on a motor vehicle**

*vehicle* means any description of vehicle on wheels and includes the vehicle's equipment and any substances that the vehicle is carrying that are essential for its operation.

*vehicle registration authority* in relation to a vehicle, means:

- (a) the authority that last registered the vehicle, or
- (b) if the vehicle has never been registered—the authority responsible for registering vehicles in the State or Territory in which the vehicle is used or is intended to be used.

*warning light* means a light that complies with the specifications in Part 6 of Schedule 2.

*warning sign* means a sign that complies with Part 7 of Schedule 2.

*wheel* includes any group of wheels which are mounted on an axle and are on one side of the longitudinal centre line of the vehicle.

*wheelbase* in relation to a vehicle, means the distance from the centre line of the vehicle's foremost axle to the rear overhang line.

*wheel load* means the mass transmitted to the road by any wheel.